

CITY OF LA VISTA
MAYOR AND CITY COUNCIL REPORT
OCTOBER 15, 2019 AGENDA

Subject:	Type:	Submitted By:
APPLICATION FOR PUD SITE PLAN – LOTS 19-20, SOUTHPORT WEST & LOTS 2-3 SOUTHPORT WEST REPLAT 2 (NW OF WESTPORT PKWY)	◆ RESOLUTION ORDINANCE RECEIVE/FILE	CHRISTOPHER SOLBERG SENIOR PLANNER

SYNOPSIS

A public hearing has been scheduled and a resolution prepared to approve a PUD Site Plan Amendment to allow for the construct three multi-tenant commercial strip centers and one stand-alone restaurant, located on approximately 3.29 acres in Southport West.

FISCAL IMPACT

None.

RECOMMENDATION

Approval, subject to completion of all requirements and conditions specified in the Planning Division Recommendation Report included with this agenda item.

BACKGROUND

A public hearing has been scheduled to consider an application submitted by Heritage Westwood La Vista, LLC for a PUD Site Plan Amendment to allow for the construction of three multi-tenant commercial strip centers and one stand-alone restaurant on approximately 3.29 acres platted as Lots 19-20, Southport West and Lot 2-3 Southport West Replat Two. The site is located northwest of the intersection Giles Road and Southport Parkway, south of the Embassy Suites hotel.

The original PUD Site Plan for this property was approved on December 21, 2004. The property lies within the Gateway Corridor Overlay District as well as Southport West and is subject to the building design review process. The applicant has submitted the building design for staff review, which is ongoing. Design review must be completed prior to the issuance of a building permit, along with completion of other specified requirements and conditions.

A detailed staff report is attached.

The Planning Commission held a public hearing on September 19, 2019 and voted unanimously to recommend approval of the PUD Site Plan for a commercial development contingent on satisfactory resolution of the issues stated within the staff report prior to City Council approval, as the PUD Site Plan request is consistent with the Comprehensive Plan and the Zoning Ordinance.

RESOLUTION NO.

A RESOLUTION OF THE MAYOR AND CITY COUNCIL OF THE CITY OF LA VISTA, NEBRASKA, APPROVING A PLANNED UNIT DEVELOPMENT (PUD) SITE PLAN AMENDMENT FOR LOTS 19-20, SOUTHPORT WEST, AND LOTS 2-3 SOUTHPORT WEST REPLAT TWO, LOCATED IN THE SW 1/4 OF SECTION 18, T14N, R12E OF THE 6TH P.M., SARPY COUNTY, NEBRASKA.

WHEREAS, the applicant, Heritage Westwood La Vista, LLC, has made an application for approval of a PUD Site Plan Amendment for Lots 19-20 Southport West, and Lots 2-3 Southport West Replat Two; and

WHEREAS, the Senior Planner and the City Engineer have reviewed the PUD Site Plan Amendment; and

WHEREAS, the La Vista Planning Commission held a public hearing on September 19, 2019 and voted unanimously to recommend approval of the PUD Site Plan Amendment; and

WHEREAS, the PUD Site Plan Amendment request is consistent with the Comprehensive Plan and the Zoning Ordinance;

NOW THEREFORE, BE IT RESOLVED by the Mayor and City Council of the City of La Vista, Nebraska, that the PUD Site Plan Amendment presented at this meeting for Lots 19-20 Southport West, and Lots 2-3 Southport West Replat Two, located in the Southwest 1/4 of Section 18, T14N, R12E of the 6th P.M., Sarpy County, Nebraska, generally located north of Westport Parkway between Giles Road and S. 125th Street, be, and hereby is, approved, subject to completion of all requirements and conditions specified in the Planning Division Recommendation Report included with this agenda item to the satisfaction of the City Administrator, City Engineer, or her or his designee.

PASSED AND APPROVED THIS 15TH DAY OF OCTOBER, 2019.

CITY OF LA VISTA

ATTEST:

Douglas Kindig, Mayor

Pamela A. Buethe, CMC
City Clerk



**CITY OF LA VISTA
PLANNING DIVISION**

RECOMMENDATION REPORT

CASE NUMBER: PPUD-19-0001

For Hearing of: October 15, 2019
Report Prepared on: October 9, 2019

I. GENERAL INFORMATION

A. APPLICANT: Heritage Westwood La Vista LLC

B. PROPERTY OWNER(S):

Heritage Westwood LLC
450 Lexington Ave
New York, NY 10017

C. LOCATION: Northwest of the intersection of Giles Road and Southport Parkway.

D. LEGAL DESCRIPTION: Lots 19-20 Southport West, together with Lots 2-3 Southport West Replat Two

E. REQUESTED ACTION(S):

Planned Unit Development (PUD) Site Plan amendment to allow for the development of three commercial strip centers and one stand-alone restaurant, including two fast food establishments with drive-thrus.

F. EXISTING ZONING AND LAND USE:

C-3 Highway Commercial/Office Park District, Gateway Corridor District (Overlay District), and Southport West PUD District (Overlay District); the property is currently vacant.

G. PROPOSED USES:

Developer wishes to construct three multi-tenant commercial strip centers and one stand-alone restaurant, including two fast food establishments with drive-thrus.

H. SIZE OF SITE: 3.29 Acres.

II. BACKGROUND INFORMATION

A. EXISTING CONDITION OF SITE: The existing site is vacant ground. There is a slight downward grade toward the southeast.

B. GENERAL NEIGHBORHOOD/AREA LAND USES AND ZONING:

1. **North:** Embassy Suites; C-3 Highway Commercial/Office Park District, Gateway Corridor District (Overlay District), and Southport West PUD District (Overlay District)
2. **West:** Cabela's; C-3 Highway Commercial/Office Park District, Gateway Corridor District (Overlay District), and Southport West PUD District (Overlay District)
3. **South:** Vacant; C-3 Highway Commercial/Office Park District, Gateway Corridor District (Overlay District), and Southport West PUD District (Overlay District)
4. **East:** Pinnacle Bank; C-3 Highway Commercial/Office Park District, Gateway Corridor District (Overlay District), and Southport West PUD District (Overlay District)

C. RELEVANT CASE HISTORY:

1. The PUD Plan and Ordinance for Southport West was originally approved on December 21, 2004.
2. The PUD Ordinance for Southport West was last amended on February 16, 2016.
3. Southport West Replat 2 was approved on June 6, 2006.

III. ANALYSIS

A. COMPREHENSIVE PLAN: The Future Land Use Map of the Comprehensive Plan designates this property for commercial uses.

B. OTHER PLANS: N/A

C. TRAFFIC AND ACCESS:

1. Access to the property will be provided from a right-in/right-out off of Southport Parkway as well as connections through the Embassy Suites parking lot.
2. Applicant has provided a traffic impact study in relation to the development of this project. The study, dated July 2019, finds that traffic is generally anticipated to operate with acceptable delay. However, several turning movements at the intersection of Southport Parkway and Giles Road are anticipated to approach or exceed capacity in short-term future conditions. The intersection of Southport Parkway and Giles Road is expected to have overall level of service "D" in the AM peak and overall level of service "E" in the PM peak with and without the project.
3. The traffic study recommends the addition of a westbound right-turn lane be constructed at the relocated access break for the Site Drive &

onto Southport Parkway. This is depicted in the PUD Plan Set and will be required at the time of construction.

4. Applicant should install all access and signage improvements as recommended in Section 7 of the Traffic Impact Study.

D. UTILITIES: All utilities are available to the site.

E. PARKING REQUIREMENTS:

The plans indicate approximately 21,262 square feet of combined gross floor area between the four buildings. The PUD ordinance for Southport West requires a ratio of 4.5 stalls per 1,000 square feet of leasable gross floor area. The gross floor area proposed within the development requires 96 stalls. The parking count depicted within the PUD Site Plan set is 141 stalls, exceeding the minimum requirement by 45 stalls.

F. LANDSCAPING:

The overall landscape plan design review is substantially complete. However, minor adjustments may be warranted resulting from utility installations and the final design package for the building design process. Each adjustment will be reviewed by the City's Design Review Architect and approved by the City.

G. BUILDING DESIGN:

The building design is currently under review by the City's Design Review Architect as part of the overall building and site design package. The design review process needs to be completed prior to the issuance of a building permit.

IV. REVIEW COMMENTS:

1. The applicant has submitted the following proposed schedule of construction:
 - a. Phase 1 - Shell & Turnkey CD Preparation – January, 2020
 - b. Phase 1 break-ground - March/April 2020
 - c. Deliver to tenants October 1, 2020
 - d. Phase II – Shell and Turnkey CD Preparation – December 2020
 - e. Phase II break- ground - March 2021
 - f. Deliver to tenants October 1, 2021

Applicant has indicated that Phase 1 consists of the eastern-most multi-tenant commercial building. Phase 2 consists of the remaining buildings in the development. Staff has no issues with the proposed timeline.

2. Steve Thornburg of the Papillion Fire Department has noted that the Fire Department approves of the PUD Amendment as long as the proposed development complies with emergency responder radio coverage requirements.

This shall be verified by the Papillion Fire Department upon the completion of construction.

3. The development will need to obtain FAA approval prior to the issuance of a building permit.
4. A common area installation and maintenance agreement needs to be approved, executed and recorded prior to obtaining a building permit. Additionally, a satisfactory financial guarantee, such as a performance bond or letter of credit, will be required prior to the issuing of the first building permit in this PUD.
5. Although the engineering consultant's response letter notes adjustments to the grading plan in relation to the berthing required as per Section 8(B)(ii)(c) of the Southport West PUD, the PUD documents only depict berthing along Westport Parkway and Southport Parkway. Berthing is required along all sides "adjacent to a public street". Berthing is required along Giles Road.
6. Movement of the existing Right-In/Right-Out (RICO) access will need to be completed through an Administrative Plat process as existing plats show a right in only access between Lot 20 and Lot 3 Per Southport West Replat 2. Such replatting could also adjust the boundaries between Lot 20 and Lot 3 to prevent potential fire code issues, in addition to revising the RICO access.

V. STAFF RECOMMENDATION – PUD SITE PLAN:

Approval of the PUD Site Plan for a commercial development contingent on satisfactorily meeting the requirements stated within the staff report prior to issuance of a building permit, as the PUD Site Plan request is consistent with the Comprehensive Plan and the Zoning Ordinance.

VI. PLANNING COMMISSION RECOMMENDATION – PUD

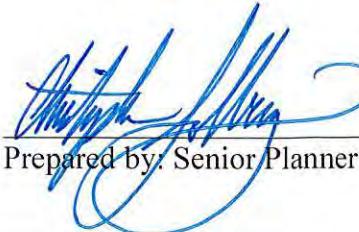
The Planning Commission held a public hearing on September 19, 2019 and voted unanimously to recommend approval of the PUD Site Plan for a commercial development contingent on satisfactory resolution of the issues stated within the staff report prior to City Council approval, as the PUD Site Plan request is consistent with the Comprehensive Plan and the Zoning Ordinance.

VII. ATTACHMENTS TO REPORT:

1. Vicinity Map
2. Staff Review and Consultant Response Letters
3. Draft PUD Site Plan Map Set
4. Brixmor North Traffic Impact Study (Without Appendices)

VIII. COPIES OF REPORT TO:

1. Marc Newman, Brixmor
2. Eric Williams, Olsson Associates
3. Patrick Morgan, Slaggie Architects, Inc.
4. Public Upon Request

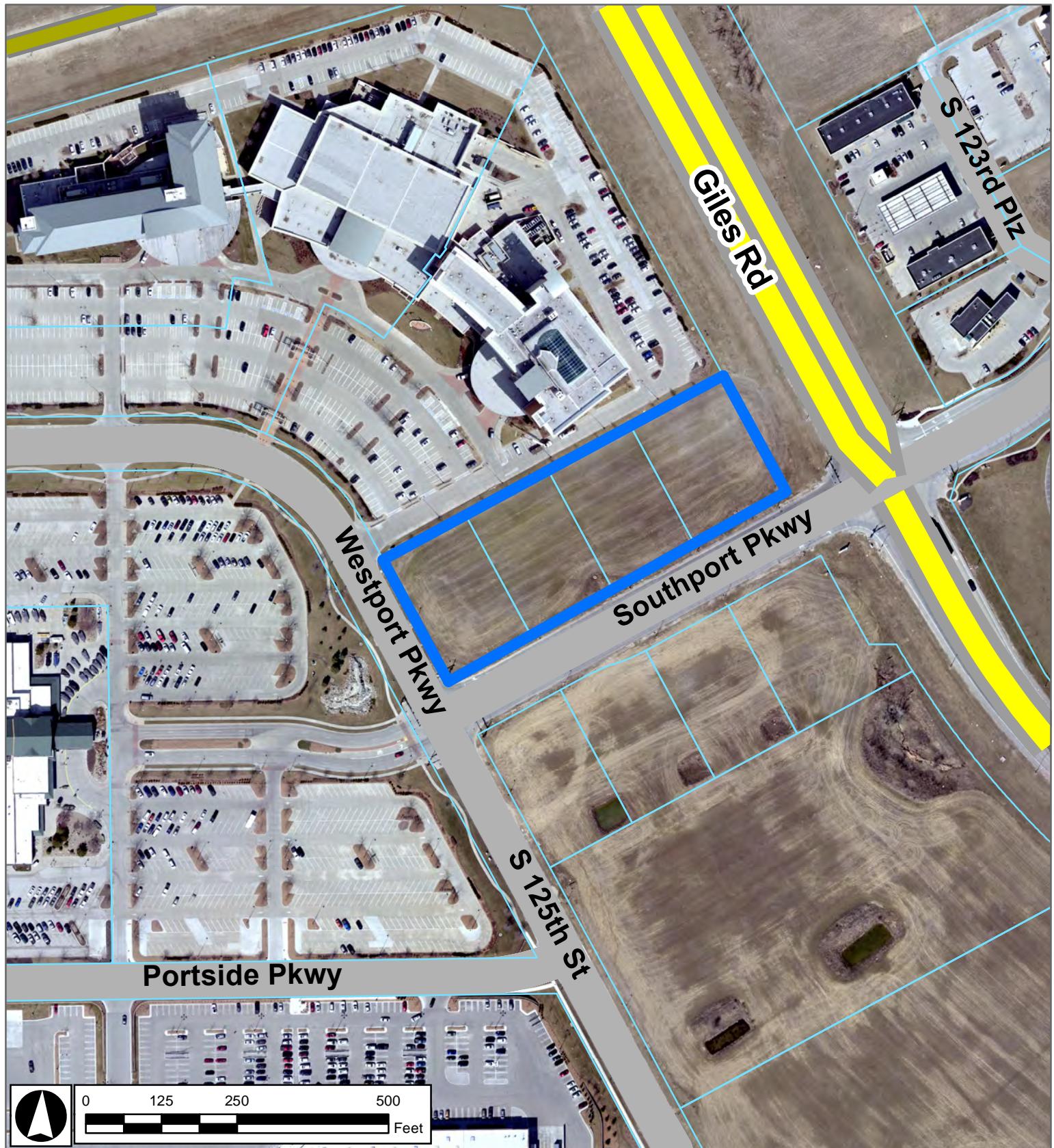


10/10/19

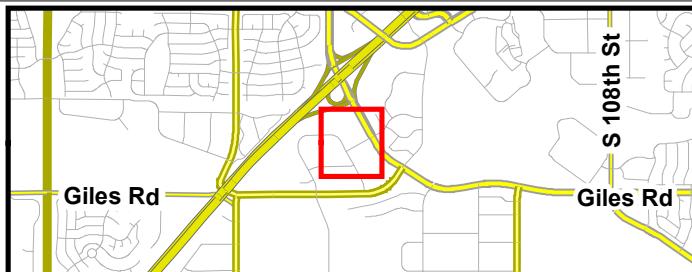
Prepared by: Senior Planner

Community Development Director

Date



Project Vicinity Map



Brixmor North PUD
Lots 19-20 Southport West, &
Lots 2-3 Southport West Replat Two

9/11/2019

CB





June 20, 2019

Marc Newman
Brixmor Property Group
450 Lexington Avenue
New York, NY 10017

RE: Planned Unit Development (PUD) Amendment
Initial Review
Brixmor North – Southport West

Mr. Newman,

We have reviewed the documents submitted for the above-referenced application. Based on the elements for consideration set forth in the applicable sections of the Zoning Ordinance, the City has the following comments:

General Comments

- Per Section 5.15.04.01, you need to submit a schedule of construction. If project phasing is planned, phasing plan and schedule needs to be included. Since there are individual lots that might be conveyed separately, there needs to be information on phasing of construction so that common area elements such as drives, storm water facilities, utilities or other common area shared items can be addressed for maintenance. If appurtenances are not all built with first phase/first lot, then financial guarantees to assure completion will be needed as part of PUD approval.
- Per Section 5.15.04.06, you need to provide evidence of meeting and coordination with the hotel property owner.
- As per Section 5.15.04.05, the application to amend the PUD needs to be signed by at least 51% of the initial PUD property owners. Provision of signed letters of support would be acceptable.

Traffic Study

- Section 5.1 and Table 5 identify General Office uses for trip generation, but Sheet C2.1 labels the building use as Retail. Please confirm use and adjust trip generation accordingly. The distribution of exiting traffic at the proposed access onto Southport Parkway at 3% seems low given pass by trips can be generated due to trips to the existing businesses within Southport.
- The proposed right-in right out (RIRO) access can suffer stacking if the first internal entrance is congested. This would be more than the two vehicles noted.

City Hall
8116 Park View Blvd.
La Vista, NE 68128-2198
p: 402-331-4343
f: 402-331-4375

Community Development
8116 Park View Blvd.
p: 402-331-4343
f: 402-331-4375

Fire
8110 Park View Blvd.
p: 402-331-4748
f: 402-331-0410

Golf Course
8305 Park View Blvd.
p: 402-339-9147

Library
9110 Giles Rd.
p: 402-537-3900
f: 402-537-3902

Police
7701 South 96th St.
p: 402-331-1582
f: 402-331-7210

Public Buildings & Grounds
8112 Park View Blvd.
p: 402-331-4343
f: 402-331-4375

Public Works
9900 Portal Rd.
p: 402-331-8927
f: 402-331-1051

Recreation
8116 Park View Blvd.
p: 402-331-3455
f: 402-331-0299

Lengthening of the taper and/or lane storage may reduce the stacking effects to Southport Parkway. A delivery truck might need to queue in this bay waiting for vehicles to clear to enter the site for deliveries.

- Pushing the Southport Parkway drive access drive east will give drivers much less room to maneuver over to access turn lane from the inside northbound to westbound left, in a pass-by situation, which may cause unforeseen vehicle conflicts. Consideration to shifting the access farther of way from the Giles Road intersection should be considered.
- The traffic study does not identify the potential for hotel related traffic to use the proposed access. Signage should be installed to direct hotel traffic to the Westport Drive access. There should also be signage at the drive connections to the hotel driveway noting no access to Giles Road. One-way signage on the island opposite the new access will be needed and No U-Turn signage at Southport & Westport intersection will be needed.
- The traffic study should discuss the on-site circulation at the proposed drive-up windows and identify improvements to accommodate the proposed movements.
- Olsson prepared signal timings for the Westport Parkway & Southport Parkway traffic signal as part of the improvement plans prepared for the City with the Costco project. The study needs to identify if signal timings need modification.

Landscaping Sheet

- Ordinance 1267 requires each lot to have 25% of each lot as open space and parking areas to include green space equal to at least 10% of the total paved area. The Landscaping plan notes 27% greenspace of the total of the four lots. This area needs to be illustrated with hatching or shading. It does not appear that each lot would individually meet the 25%, therefore language may be needed to address this in the PUD Amendment. Data on the parking lot greenspace is needed.
- Confirm all landscaping/berm requirements are being met per current PUD, unless otherwise noted within a proposed PUD amendment.

Sheet C1.1

- The existing storm sewer and landscaping easements from the existing previous plats need to be noted.
- Any removals of existing infrastructure or landscaping should be noted on this sheet.

- The RIRO access change will most likely need to be done through a replatting process as existing plats show a right in only access at the between lots 20 and 3 Per Southport West Replat 2. Replatting could also adjust the boundaries between lot 20 and 3 and perhaps address any setback and/or fire code issues, as well as add notation for the RIRO access.

Sheet C2.1

- Proposed public sidewalk needs to be 6 inches thick and not closer than 6 feet to back of curb.
- Pavement thicknesses in the Site Key Notes need to be filled in.
- Need dual ADA ramps at Southport Parkway and Westport Parkway intersection (currently only one ramp is shown)
- Walk connections to hotel not shown
- Walk connections to public sidewalk not shown
- Traffic circulation to the buildings with drive-up windows needs clarified, appears that one-way traffic may be intended but not clearly shown.
- The drive-up lane at rear of bldg. on Lot 2 needs barrier from inbound traffic to avoid confusion/accidents
- Parallel parking stalls in front of 4,844 sf building are too small and will potentially add congestion. Confirm parking count is adequate per building use.
- Radii at the proposed RIRO access on the interior turns should be greater.
- The Site Information Table lists the existing and proposed zoning as "MU". Both should be listed as "C-3 PUD with a Gateway Corridor Overlay"
- Ordinance 1267 states that building setbacks shall meet C-3 zoning unless otherwise approved by City Council as part of the final PUD. The setbacks in the site information table and on the map of C2.1 need to depict the setbacks set forth within Section 5.12.06.01 of the Zoning Ordinance. Specifically,

Front Yard Setback:	25'
Front Yard Setback (when parking present in front yard):	50'
Side Yard Setback:	15'
Rear Yard Setback:	15'

The building setback dimension along Giles Road and Southport Parkway needs to be adjusted to 50 feet; 25 feet along Westport Parkway; and 15 feet along the property line shared with the hotel.

- Proposed building on Lot 20 does not meet the 15 ft side yard setback of the C-3 District. Dimension proposed setbacks from the side yard lines.
- Building coverage does not appear to be an issue, but data should be set forth on the site layout plan to show compliance with the 60% limitation to conform with Article 5.15.04.10.
- Proposed locations for potential project directory or center identification signs need to be shown

Sheet C3.1

- Grading work will require a grading permit. Refer to the Master Fee Ordinance for details.
- Grading plan needs to address a conceptual design of water quality and 2-year peak flow detention, such as indication of bio-basins or under parking lot facilities.
- As there are drainage capacity limitations at the culvert crossing near under the rail road southeast of Giles road, all catchments drain that drain into said basin should be held to a no-net increase of the predevelopment flows until the drainage culverts at the railroad is up-sized to account for post-developed conditions for a 100-year return frequency.
- The grading plan submitted does not depict the required berthing of the landscaped areas along Westport Parkway as required per Section 8(B)(ii)(c) of the Southport West PUD.

Sheet C4.1

- Please confirm that the water main on the hotel lot proposed for connection is a public water main.
- The proposed sanitary sewer is running against the topography to reach Westport Parkway. There may be stub outs in Southport Parkway that could be able to serve the lots.
- PCSMP Plan will need to be reference to this drawing as well.
- If the existing storm sewer at the northeasterly side of Lot 2 is outside of the 15 ft easement as it is shown, additional easement will be required.

A separate Emergency Vehicle Access Plan which includes Site Plan, Fire Lane, Fire Hydrants, PIV, and FDC - Use Auto-Turn Professional, or similar program, using specifications provided by Fire Marshal.

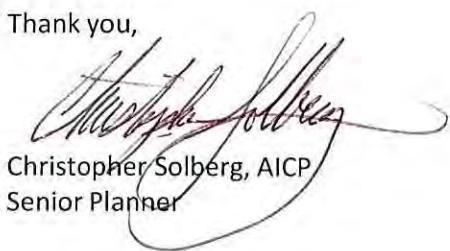
The building, site design, and the landscape plan are currently under review by the City's Design Review Architect. A separate design review letter will be forwarded once the initial review has been completed. The design review process needs to be substantially complete prior to proceeding through City Council approval process.

Note that the development will need to obtain FAA approval prior to the issuance of a building permit. Also, there may be private, protective covenants recorded against the property that the applicant should review.

In order for the PUD to be considered for review at the July 18th Planning Commission meeting, revised documents will need to be provided for review. Please submit 4 full size copies (along with electronic copies) of the required documents by noon on June 26, 2019 to ensure that the application stays on track for the review by the Planning Commission in July. If the issues within this letter are not resolved sufficiently with the next submittal, the submittal of the application to the Planning Commission for review will be laid over until the August Planning Commission meeting.

If you have any questions regarding these comments please feel free to contact me at any time.

Thank you,



Christopher Solberg, AICP
Senior Planner

cc: John Kottmann, City Engineer
Pat Dowse, City Engineer
Eric Williams, Olsson
Patrick Morgan, Slaggie Architecture

Comment Response: PUD Brixmor North

General Comments

- Per Section 5.15.04.01, you need to submit a schedule of construction. If project phasing is planned, phasing plan and schedule needs to be included. Since there are individual lots that might be conveyed separately, there needs to be information on phasing of construction so that common area elements such as drives, storm water facilities, utilities or other common area shared items can be addressed for maintenance. If appurtenances are not all built with first phase/first lot, then financial guarantees to assure completion will be needed as part of PUD approval.
Response: Schedule of construction will be provided by the owner.
- Per Section 5.15.04.06, you need to provide evidence of meeting and coordination with the hotel property owner. **Response: The owner has been in contact with the hotel and will provide evidence of this coordination.**
- As per Section 5.15.04.05, the application to amend the PUD needs to be signed by at least 51% of the initial PUD property owners. Provision of signed letters of support would be acceptable.
Response: Sign off from the property owners will be provided once completed.

Traffic Study

- Section 5.1 and Table 5 identify General Office uses for trip generation, but Sheet C2.I labels the building use as Retail. Please confirm use and adjust trip generation accordingly. The distribution of exiting traffic at the proposed access onto Southport Parkway at 3% seems low given pass by trips can be generated due to trips to the existing businesses within Southport.
Response: The general office land (LUC 710) use was updated to LUC 820 Shopping Center as a general retail land use for the development. In addition, the pass-by trip distribution and trips were updated. [See Section 5.1 and 5.2 of TIS]
- The proposed right-in right out (RIRO) access can suffer stacking if the first internal entrance is congested. This would be more than the two vehicles noted. **Response: The issue of internal congestion was added to the report and adding 25 feet to the turn lane is proposed (75 feet storage plus taper – allowing for 3 vehicles to stack in turn lane). Delivery truck traffic is being proposed to be directed to use the Westport entrance to have less potential impact on Southport operations (similar for hotel traffic in comment 4). [See Section 5.3 of TIS]**
- Pushing the Southport Parkway drive access drive east will give drivers much less room to maneuver over to access turn lane from the inside northbound to westbound left, in a pass-by situation, which may cause unforeseen vehicle conflicts. Consideration to shifting the access farther off of way from the Giles Road intersection should be considered. **Response: Based on coordination with John Kottmann with the City of La Vista and the client during site plan concept meetings, the location of the access was noted to be at the current location shown on the plans. Language was added to the report to note this. [See Section 5.0 of TIS]**
- The traffic study does not identify the potential for hotel related traffic to use the proposed access. Signage should be installed to direct hotel traffic to the Westport Drive access. There should also be

signage at the drive connections to the hotel driveway noting no access to Giles Road. One-way signage on the island opposite the new access will be needed and No U-Turn signage at Southport & Westport intersection will be needed. **Response: Signage will be proposed to direct hotel traffic (and delivery traffic) to use Westport Parkway to reduce potential congestion at the Southport drive. [See Section 5.3 and 7.0 of TIS]**

- The traffic study should discuss the on-site circulation at the proposed drive-up windows and identify improvements to accommodate the proposed movements. **Response: Language added to the report to describe available stacking distances for each drive-through window. [See Section 5.3 of TIS]**
- Olsson prepared signal timings for the Westport Parkway & Southport Parkway traffic signal as part of the improvement plans prepared for the City with the Costco project. The study needs to identify if signal timings need modification. **Response: Signal timings at Westport Parkway & Southport Parkway were updated. Note that only weekday PM and Saturday signal timings were analyzed in the Costco project – therefore only PM signal timings from Costco project were used in this study. There are no recommended modifications to these timings. [See Section 3.2 and 7.0 of TIS]**

Landscaping Sheet

- Ordinance 1267 requires each lot to have 25% of each lot as open space and parking areas to include green space equal to at least 10% of the total paved area. The Landscaping plan notes 27% greenspace of the total of the four lots. This area needs to be illustrated with hatching or shading. It does not appear that each lot would individually meet the 25%, therefore language may be needed to address this in the PUD Amendment. Data on the parking lot greenspace is needed. **Response: Landscaping Plans have been updated with the requested table.**
- Confirm all landscaping/berm requirements are being met per current PUD, unless otherwise noted within a proposed PUD amendment. **Response: Berms have been added along Westport Parkway and shrubs are provided along Southport for screening.**

Sheet C1.1

- The existing storm sewer and landscaping easements from the existing previous plats need to be noted. **Response: This has been added to the plan.**
- Any removals of existing infrastructure or landscaping should be noted on this sheet. **Response: Removals have been added to the plans.**
- The RIRO access change will most likely need to be done through a replatting process as existing plats show a right in only access at the between lots 20 and 3 Per Southport West Replat 2. Replatting could also adjust the boundaries between lot 20 and 3 and perhaps address any setback and/or fire code issues, as well as add notation for the RIRO access. **Response: Understood. Follow up discussion is needed to determine what is requested for this replat.**

Sheet C2.1

- Proposed public sidewalk needs to be 6 inches thick and not closer than 6 feet to back of curb. **Response: Notes have been revised.**

- Pavement thicknesses in the Site Key Notes need to be filled in. **Response: This has been completed.**
- Need dual ADA ramps at Southport Parkway and Westport Parkway intersection (currently only one ramp is shown). **Response: Ramps have been revised.**
- Walk connections to hotel not shown. **Response: Connection to the hotel has been added.**
- Walk connections to public sidewalk not shown. **Response: Connection to public sidewalk have been provided.**
- Traffic circulation to the buildings with drive-up windows needs clarified, appears that one-way traffic may be intended but not clearly shown. **Response: The site has been modified to provide a 3' island for separation of drive-thru traffic and circulating traffic.**
- The drive-up lane at rear of bldg. on Lot 2 needs barrier from inbound traffic to avoid confusion/accidents. **Response: The site has been modified to provide a 3' island for separation of drive-thru traffic and circulating traffic.**
- Parallel parking stalls in front of 4,844 sf building are too small and will potentially add congestion. Confirm parking count is adequate per building use. **Response: This parking has been removed and parking counts provided.**
- Radii at the proposed RIRO access on the interior turns should be greater. **Response: This has been revised.**
- The Site Information Table lists the existing and proposed zoning as "MU". Both should be listed as "C-3 PUD with a Gateway Corridor Overlay". **Response: This has been revised.**

Ordinance 1267 states that building setbacks shall meet C-3 zoning unless otherwise approved by City Council as part of the final PUD. The setbacks in the site information table and on the map of C2.1 need to depict the setbacks set forth within Section 5.12.06.01 of the Zoning Ordinance. Specifically, The building setback dimension along Giles Road and Southport Parkway needs to be adjusted to 50 feet; 25 feet along Westport Parkway; and 15 feet along the property line shared with the hotel. **Response: Setbacks along the roadways have been revised and noted.**

- Proposed building on Lot 20 does not meet the 15 ft side yard setback of the C- 3 District. Dimension proposed setbacks from the side yard lines. **Response: Per conversations with staff, this is an interior lot line and would not need to meet this requirement. *"In review of the regulations, Section 5.15.04.16 will allow for up to a zero lot line setback for lot lines that are interior to the development. Hence, an Administrative Plat will not be required."***
- Building coverage does not appear to be an issue, but data should be set forth on the site layout plan to show compliance with the 60% limitation to conform with Article 5.15.04.10. **Response: A table has been provided with the building coverage.**
- Proposed locations for potential project directory or center identification signs need to be shown. **Response: Directory sign location has been noted within the existing signage easement.**

Sheet C3.1

- Grading work will require a grading permit. Refer to the Master Fee Ordinance for details. **Response: Grading permit will be obtained during the permit process.**
- Grading plan needs to address a conceptual design of water quality and 2-year peak flow detention, such as indication of bio-basins or under parking lot facilities. **Response: A water quality structure has been provided along with under parking storage to ensure we are not increasing the offsite flow.**
- As there are drainage capacity limitations at the culvert crossing near under the rail road southeast of Giles road, all catchments drain that drain into said basin should be held to a no-net increase of the predevelopment flows until the drainage culverts at the railroad is up-sized to account for post-developed conditions for a 100-year return frequency. **Response: A water quality structure has been provided along with under parking storage to ensure we are not increasing the offsite flow.**
- The grading plan submitted does not depict the required berthing of the landscaped areas along Westport Parkway as required per Section 8(B)(ii)(c) of the Southport West PUD. **Response: Berthing has been added and noted on the plan.**

Sheet C4.1

- Please confirm that the water main on the hotel lot proposed for connection is a public water main. **Response: We have coordinated with MUD for the main locations and revised the plan to connect to the main in Westpoint Parkway.**
- The proposed sanitary sewer is running against the topography to reach Westport Parkway. There may be stub outs in Southport Parkway that could be able to serve the lots. **Response: Based on GIS, there are no stubs in Southport Parkway to serve the lots.**
- PCSMP Plan will need to be reference to this drawing as well. **Response: PCSMP note has been referenced.**
- If the existing storm sewer at the northeasterly side of Lot 2 is outside of the 15 ft easement as it is shown, additional easement will be required. **Response: Based on survey, this line appears to be just inside of the easement line and has been modified accordingly.**

A separate Emergency Vehicle Access Plan which includes Site Plan, Fire Lane, Fire Hydrants, PIV, and FDC - Use Auto-Turn Professional, or similar program, using specifications provided by Fire Marshal. **Response: A fire access plan has been provided with the submittal.**

July 31, 2019



Marc Newman
Brixmor Property Group
450 Lexington Avenue
New York, NY 10017

RE: Planned Unit Development (PUD) Amendment
Second Review
Brixmor North – Southport West

Mr. Newman,

We have reviewed the documents submitted for the above-referenced application. Based on the elements for consideration set forth in the applicable sections of the Zoning Ordinance, the City has the following comments:

General Comments

- Per Section 5.15.04.01, Applicant needs to submit a schedule of construction as discussed in the previous review letter. In discussions between the Applicant's consulting engineer and Engineering Department staff, it appears that all private improvements will be completed with the construction of the first lot. City will need assurances that installing/encumbering/providing necessary common improvements will take place with the construction of the initial lot.
- Per Section 5.15.04.06, Applicant needs to provide evidence of meeting and coordination with the hotel property owner. This has yet to be provided.
- As per Section 5.15.04.05, the application to amend the PUD needs to be signed by at least 51% of the initial PUD property owners. Signed letters of support have been provided. However, calculations need to be provided that the three entities received make up the 51%+ necessary.

Traffic Study

- Please note the source of Existing ADT values in Figure 2 of the study.
- Please provide total driveway volumes when considering both primary and pass-by trips.
- Trip distribution does not appear to fully take into account pass by trips to destinations within Southport development. Is there a standard methodology that is being used to apply the distribution percentages as applicable adjacent land uses/trip generators?

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f: 402-331-4375

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f: 402-331-0410

Golf Course
8305 Park View Blvd.
p: 402-339-9147

Library
9110 Giles Rd.
p: 402-537-3900
f: 402-537-3902

Police
7701 South 96th St.
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Public Buildings & Grounds
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f: 402-331-0299

- AM signal timings should be reviewed to make recommendations as to any pertinent modifications.
- Applicant should install all access and signage improvements as recommended in Section 7 of the Traffic Impact Study.

Sheet L1.1

- Confirm all landscaping/berm requirements are being met per current PUD ordinance.

Sheet C2.1

- The traffic flow around the building in Lot 19 is not clearly indicated; appears the traffic is intended to travel one way.
- Applicant needs one additional walking connection north to the hotel that lines up with the side door to the building. Confirm with hotel site plan.
- Although developments are provided some flexibility in regards to setbacks through the use of a PUD, the Planning Commission and City Council will need to know what revised setbacks they are reviewing. Hence, the proposed side yard setbacks for the buildings planned for Lot 3 and Lot 20 need to be adequately dimensioned on the PUD Site Plan.
- Confirm adequate parking for Lot 2 given the proposed use(s) of the building.
- The building setback dimension along Southport Parkway still needs to be adjusted to 50 feet.
- The site plan depicts a “monument sign” in the southeast corner of the development. Applicant needs to clarify if this is the type of sign requested to be used. If a multi-tenant style sign is planned, the labeling needs to be changed to “Project Directory” or “Center Identification”. Please note the spacing requirements for Center Identification signage in Article 7 of the zoning ordinance when considering the signage plan and related impacts on future development of the property on the south side of Southport Parkway.
- Setbacks for the signage based on the sign type mentioned above and the PUD ordinance need to be depicted for the sign in the southeast corner of the development.

Sheet C3.1

- Underground storage is an acceptable method for post construction requirement, however it is unknown what sizing parameters were used. Sizing should be confirmed within the drainage report.

- Although the engineering consultant's response letter notes adjustments to the grading plan in relation to the berthing required as per Section 8(B)(ii)(c) of the Southport West PUD, the PUD documents submitted do not sufficiently depict the required berthing.

Sheet C4.1

- City provided to the Applicant's engineering consultant a scan of what is believed to be the as-built locations of the sanitary sewer stubs in Southport Parkway, to which each lot should have a stub out to Southport Parkway.
- Per the engineering consultant's response letter, the storm sewer on the northeasterly side of Lot 2 is stated to be within an existing easement. However, it does not appear so in the revised plans. The easement would appear to need to be modified.

Sheet C5.1

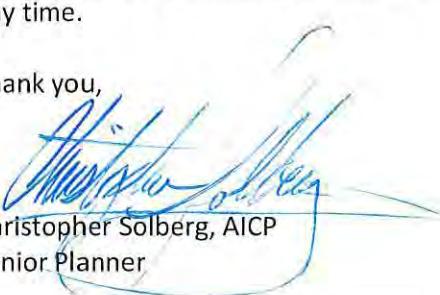
- The Fire Marshall requests to see how trucks can maneuver throughout the site, not just between the entrances to the development.

The building, site design, and the landscape plan are currently under review by the City's Design Review Architect. A separate design review letter is expected by the end of this week and will be forwarded once the initial review has been completed. The design review process needs to be substantially complete prior to proceeding through City Council approval process.

Revised documents will need to be provided for review. Please submit 4 full size copies (along with electronic copies) of the required documents as soon as possible to ensure that the application stays on track for the review by the Planning Commission in September.

If you have any questions regarding these comments please feel free to contact me at any time.

Thank you,


Christopher Solberg, AICP
Senior Planner

cc: John Kottmann, City Engineer
Pat Dowse, City Engineer
Eric Williams, Olsson
Patrick Morgan, Slaggie Architecture



Comment Response: PUD Brixmor North

General Comments

- Per section 5.15.04.01, Applicant needs to submit a schedule of construction as discussed in the previous review letter. In discussions between the Applicant's consulting engineer and Engineering Department staff, it appears that all private improvements will be completed with the construction of the first lot. **Response: Based on updated utility information, the lots each now will have their own separate water and sewer service which would need to be extended as the lots come are ready for construction. The turn lane and full drive width for shared access will be provided when the first lot is constructed.**
- Per Section 5.15.04.06, you need to provide evidence of meeting and coordination with the hotel property owner. **Response: The owner has been in contact with the hotel JDHQ Hotels/Atrium Hospitality – the owner of record and has provided the city both the email evidencing coordination with JDHQ as well as a letter evidencing the JDHQ's consent to the project.**
- As per Section 5.15.04.05, the application to amend the PUD needs to be signed by at least 51% of the initial PUD property owners. Provision of signed letters of support would be acceptable. **Response: The owner has provided the city with letters of consent of 54% of the PUD ownership.**

Traffic Study

- Please note the source of Existing ADT values in Figure 2 of the study **Response:**
- Please provide total driveway volumes when considering both primary and pass-by trips. **Response:**
- AM signal timings should be reviewed to make recommendations as to any pertinent modifications **Response:**
- Applicant should install all access and signage improvements as recommended in Section 7 of the Traffic Impact Study **Response:**

Landscaping Sheet

- Confirm all landscaping/berm requirements are being met per current PUD ordinance **Response: Landscaping Plans have been revised to add the requested berms coordinated with Civil sheets.**

Sheet C2.1

- The traffic flow around the building in Lot 19 is not clearly indicated; appears the traffic is intended to travel one way
Response: The traffic is intended to be one-way traffic. Direction arrows and signage have been added.
- Applicant needs one additional walking connection north to the hotel that lines up with the side door to the building. Confirm with hotel site plan
Response: This has been added and coordinated with hotel site configuration.
- Although developments are provided some flexibility in regards to setbacks through the use of a PUD, the Planning Commission and City Council will need to know what revised setbacks they are reviewing. Hence, the proposed side yard setbacks for the buildings planned for Lot 3 and Lot 20 need to be adequately dimensioned on the PUD Site Plan.
Response: Dimensions for the internal setbacks have been provided.
- Confirm adequate parking for Lot 2 given the proposed use(s) of the building
Response: A breakout of parking per lot has been provided.
- The building setback dimension along Southport Parkway still needs to be adjusted to 50 feet.
Response: Setbacks have been adjusted on the plan.
- The site plan depicts a “monument sign” in the southeast corner of the development. Applicant needs to clarify if this is the type of sign requested to be used. If a multi-tenant style sign is planned, the labeling needs to be changed to “Project Directory” or “Center Identification”. Please note the spacing requirements for Center Identification signage in Article 7 of the zoning ordinance when considering the signage plan and related impacts on future development of the property on the south side of Southport Parkway.
Response: This has been revised to reflect a Project Directory signage note.
- Setbacks for the signage based on the sign type mentioned above and the PUD ordinance need to be depicted for the sign in the southeast corner of the development.
Response: Setbacks have been updated. Signage has not been designed for the development.

Sheet C3.1

- Underground storage is an acceptable method for post construction requirement, however it is unknown what sizing parameters were used. Sizing should be confirmed within the drainage report.
Response: Sizing of the underground storage will be provided with the drainage report provided with the construction drawings. Preliminary calculations through ADS has noted 2 tanks with a minimum length of 112' per run with 2 tanks is required to separate out the 2 drainage areas.
- Although the engineering consultant’s response letter notes adjustments to the grading plan in relation to the berthing required as per Section 8(B)(ii)(c) of the Southport West PUD, the PUD documents submitted do not sufficiently depict the required berthing.
Response: Berthing has been identified and coordinated with the landscape plan.

Sheet C4.1

- City provided to the Applicant's engineering consultant a scan of what is believed to be the as-built locations of the sanitary sewer stubs in Southport Parkway, to which each lot should have a stub out to Southport Parkway.

Response: The sanitary has been updated to reflect the information kindly provided from the City.

- Per the engineering consultant's response letter, the storm sewer on the northeasterly side of Lot 2 is stated to be within an existing easement. However, it does not appear so in the revised plans. The easement would appear to need to be modified.

Response: With the survey information we have for the site, it appears the line possibly veers from the existing easement. An Easement will be provided by separate instrument for this storm outside of the easement. (attached)

Sheet C5.1

- The Fire Marshall requests to see how trucks can maneuver throughout the site, not just between the entrances to the development.

Response: Routing has been updated through the site.

September 6, 2019



Marc Newman
Brixmor Property Group
450 Lexington Avenue
New York, NY 10017

RE: Planned Unit Development (PUD) Amendment
Third Review
Brixmor North – Southport West

Mr. Newman,

We have reviewed the documents submitted for the above-referenced application. Based on the elements for consideration set forth in the applicable sections of the Zoning Ordinance, the City has the following comments:

General Comments

- Per Section 5.15.04.01, Applicant needs to submit a schedule of construction as discussed in the previous review letter. This has yet to be provided.
- It appears that there are several common area elements, such as the master water service and storm water management systems in addition to the RIRO access. There needs to be an itemized estimate of construction costs for these items, and any other common area items. It will be necessary to have a satisfactory financial guarantee, such as a performance bond or letter of credit, prior to issuing the first building permit in this PUD.

Sheet L1.1

- Proposed berthing not in conformance with regulations. See comments regarding Sheet C3.1 for more information.
- The dumpster location in the northeast corner of the property needs to be moved away from the Giles Road frontage.

Sheet C2.1

- Resubmitted documents depict the proposed sign at the Southeast corner of the development as a Project Directory sign. As such, minimum setbacks from Giles Road (arterial road) and Southport Parkway (collector road) need to be 20 feet as per Section 7.01.05(6)(C) of the Zoning Ordinance. Setbacks are currently dimensioned at 10 feet in either direction, but are measured from the landscaping easement. Measuring the setbacks from the edge of property should alleviate this issue without impacting the existing sign location.

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Sheet C3.1

- Although the engineering consultant's response letter notes adjustments to the grading plan in relation to the berthing required as per Section 8(B)(ii)(c) of the Southport West PUD, the PUD documents only depict berthing along Westport Parkway and the western half of Southport Parkway.

To provide assistance in ways to meet the requirements the following clarification is provided:

- a. Although Section 7.17.03.02(3) of the Zoning Ordinance requires the planting of one tree per 40-feet, the intention isn't that it must be planted regularly "on-center". Attached to this letter is Appendix "C" that depicts grouping of the trees (though possibly too close for survivability), which opens up space for berthing.
- b. Additionally, 7.17.03.02(3) doesn't require overstory trees. Hence a mixture of overstory trees combined with understory/ornamental trees that have a smaller growth radius provides variety and additional space between trees for berthing.
- c. The recent Hooters grading and landscaping plans (attached) depict a method where the berthing is incorporated with landscaping and understory trees. This design is replicated along Westport Parkway in the Boot Barn/Shoppes at Southport area further south of Hooters.
- d. As the parking is close to the property lines, trees in the two bump-outs in the parking lot along Southport Parkway and Giles can be counted in the calculations needed to meet requirements of Section 7.17.03.02(3).

Sheet C5.1

- In the Emergency Vehicle Access Plan, the proper apparatus turning radii and proper vehicle size were not used. The proper apparatus, provided by the Fire Marshall, is attached. Please adjust the turns and display the proper basis/legend in the top-right corner.

The City's Design Review Architect is awaiting a full resubmittal of the building, site design, and the landscape plan. Due to the timing of the design review in relation to the PUD, it is important to provide complete resubmittals to ensure there are no delays in regards to City Council review of the PUD.

It has been determined that this application, dependent on the resolution of the aforementioned issues, is ready for review by the Planning Commission. Please submit 14 full size copies of the PUD exhibits for the Planning Commission packet preparation by noon on Wednesday, September 11, 2019.

The next Planning Commission meeting is Thursday, September 19, 2019 at 7:00pm. Please have someone in attendance at the meeting to provide a short presentation of the project to the Planning Commission and to answer questions as necessary.

If you have any questions regarding these comments please feel free to contact me at any time.

Thank you,



Christopher Solberg, AICP
Senior Planner

cc: John Kottmann, City Engineer
Pat Dowse, City Engineer
Eric Williams, Olsson
Patrick Morgan, Slaggie Architecture

Comment Response: PUD Brixmor North 09/09/19

General Comments

- Per Section 5.15.04.01, Applicant needs to submit a schedule of construction as discussed in the previous review letter. This has yet to be provided. **Response: Please see below for a list from the developer for planned construction:**
 - a. **Phase 1 - Shell & Turnkey CD Preparation – January, 2020**
 - b. **Phase 1 break-ground - March/April 2020**
 - c. **Deliver to Starbucks and other tenants October 1, 2020**
 - d. **Phase II – Shell and Turnkey CD Preparation – December 2020**
 - e. **Phase II break- ground - March 2021**
 - f. **Deliver to other tenants October 1, 2021**
- It appears that there are several common *area* elements, such as the master water service and storm water management systems in addition to the RIRO access. There needs to be an itemized estimate of construction costs for these items, and any other common area items. It will be necessary to have a satisfactory financial guarantee, such as a performance bond or letter of credit, prior to issuing the first building permit in this PUD. **Response: Cost Assumption provided with the submittal for the RIRO, storm/detention, water and fire lines.**

Sheet L1.1

- Proposed berthing not in conformance with regulations. See *comments* regarding Sheet C3.1 for more information. **Response: Berming has been revised to meet the requirements provided in Appendix C.**
- The dumpster location in the northeast corner of the property needs to be moved away from the Giles Road frontage. **Response: Dumpster has been relocated to the north parking area. This has been placed outside of the easement and the site plan has been updated.**

Sheet C2.1

- Resubmitted documents depict the proposed sign at the Southeast corner of the development as a Project Directory sign. As such, minimum setbacks from Giles Road (arterial road) and Southport Parkway (collector road) need to be 20 feet as per Section 7.01.05(6)(C) of the Zoning Ordinance. Setbacks are currently dimensioned at 10 feet in either direction, but are measured from the landscaping easement. Measuring the setbacks from the edge of property should alleviate this issue without impacting the existing sign location. **Response: The dimensions have been revised to be from property line instead of setback line.**

Sheet C3.1

- Although the engineering consultant's response letter notes adjustments to the grading plan in relation to the berthing required as per Section 8(B)(ii)(c) of the Southport West PUD, the PUD documents only depict berthing along Westport Parkway and the western half of

Southport Parkway.

To provide assistance in ways to meet the requirements the *following* clarification is provided:

- a. Although Section 7.17.03.02(3) of the Zoning Ordinance requires the planting of one tree per 40-feet, the intention isn't that it must be planted regularly "on-center". Attached to this letter is Appendix "C" that depicts grouping of the trees (though possibly too close for survivability), which opens up space for berthing.
- b. Additionally, 7.17.03.02(3) doesn't require overstory trees. Hence a mixture of overstory trees combined with understory/ornamental trees that have a smaller growth radius provides variety and additional space between trees for berthing.
- c. The recent Hooters grading and landscaping plans (attached) depict a method where the berthing is incorporated with landscaping and understory trees. This design is replicated *along* Westport Parkway in the Boot Barn/Shoppes at Southport area further south of Hooters.
- d. As the parking is close to the property lines, trees in the two bump-outs in the parking lot along Southport Parkway and Giles can be counted in the calculations needed to meet requirements of Section 7.17.03.02(3).

Response: Berming and landscape have been revised to meet the requirements provided in Appendix C. Counts and plans have been revised to reflect the berms and landscape/trees and sidewalks have been revised to accommodate the berthing to meet the requirements and example plans provided.

Sheet C5.1

- In the Emergency Vehicle Access Plan, the proper apparatus turning radii and proper vehicle size were not used. The proper apparatus, provided by the Fire Marshall, is attached. Please adjust the turns and display the proper basis/legend in the top-right corner. **Response: Emergency Access Plan has been updated per the approved apparatus and plan information has been updated.**

Southport West

Retail Development Lots 2, 3, 19, 20



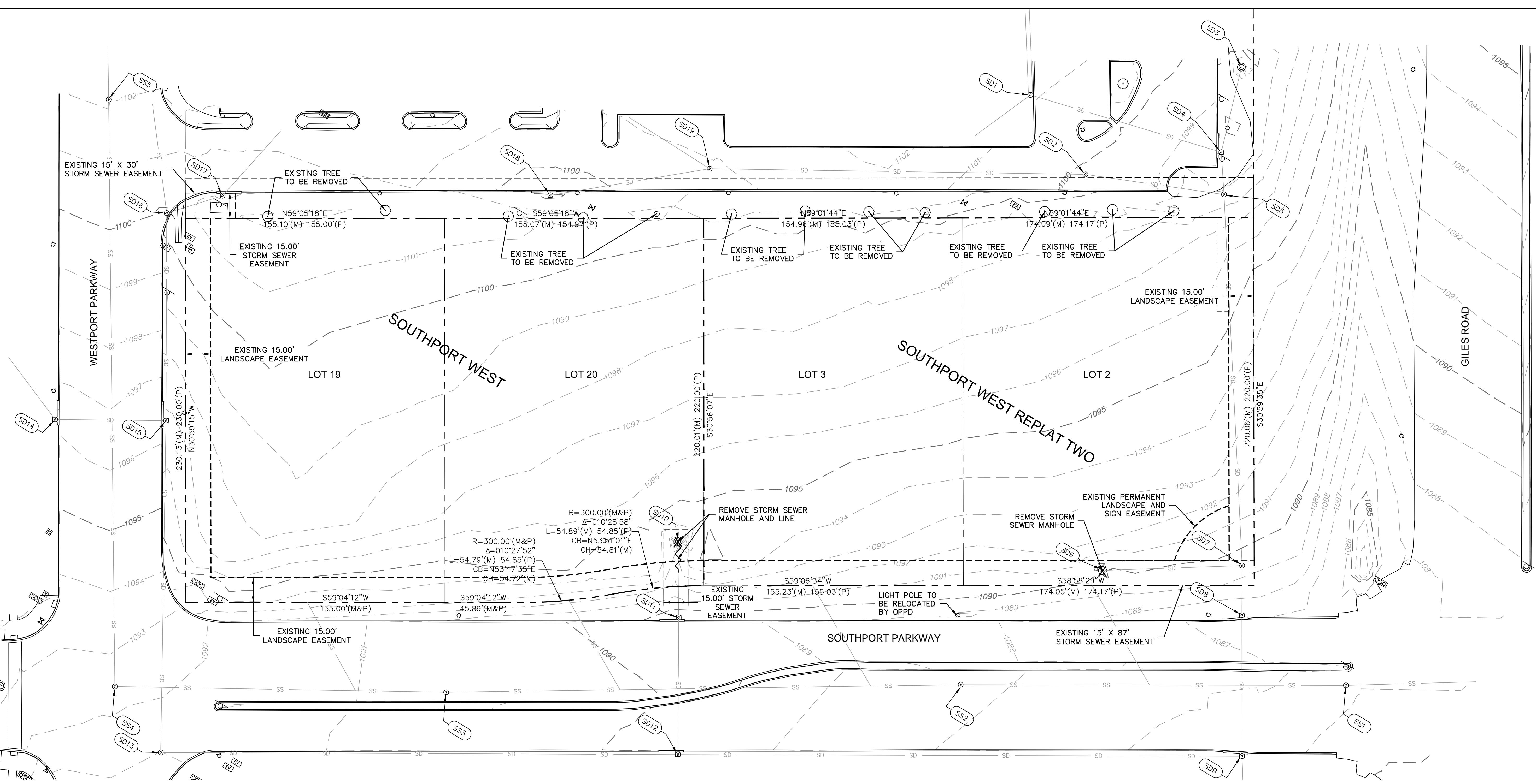
Project 11 100% 100%

RUP Researcher

August 20, 2010

CH1 - PI

- 10 -



STRUCTURE LEGEND

(SD1)	STORM SEWER MANHOLE RIM EL.=1101.38 4"CPP(N)=1098.34 8"PVC(N)=1098.43 12"CPP(SE)=1094.99 6"PVC(SW)=1098.23	(SD8)	STORM SEWER MANHOLE RIM EL.=1087.20 30"RCP(NW)=1082.23 36"RCP(SE)=1082.19	(SD15)	STORM SEWER MANHOLE RIM EL.=1096.85 30"RCP(NW)=1090.64 24"RCP(SW)=1090.91 36"RCP(SE)=1090.43	(SS1)	SANITARY MANHOLE RIM EL.=1086.31 12"PPC(SW)=1074.27 12"PPC(NE)=1074.21
(SD2)	STORM SEWER MANHOLE RIM EL.=1099.84 24"CPP(SW)=1093.50 24"CPP(SE)=1093.43	(SD9)	STORM SEWER MANHOLE RIM EL.=1087.06 36"RCP(NW)=1080.00 42"RCP(SW)=1079.56 60"RCP(SE)=1079.35	(SD16)	STORM SEWER MANHOLE RIM EL.=1099.85 24"RCP(NW)=1094.55 30"RCP(NE)=1094.18 30"RCP(SE)=1093.94	(SS2)	SANITARY MANHOLE RIM EL.=1088.29 12"PPC(SW)=1076.49 12"PPC(NE)=1076.41
(SD3)	STORM SEWER MANHOLE RIM EL.=1099.01 30"RCP(SE)=1094.32	(SD10)	STORM SEWER MANHOLE RIM EL.=1094.40 24"RCP(SE)=1086.75	(SD17)	STORM SEWER MANHOLE RIM EL.=1100.96 30"RCP(NE)=1096.91 30"RCP(SW)=1096.40	(SS3)	SANITARY MANHOLE RIM EL.=1090.66 12"PPC(SW)=1076.95 12"PPC(NE)=1076.80
(SD4)	STORM SEWER MANHOLE RIM EL.=1099.18 30"CPP(NW)=1092.58 30"CPP(SE)=1092.55 12"CPP(SE)=1092.90	(SD11)	STORM SEWER MANHOLE RIM EL.=1089.49 24"RCP(NW)=1084.88 24"RCP(SE)=1084.79	(SD18)	STORM SEWER MANHOLE RIM EL.=1100.00 24"RCP(NE)=1096.47	(SS4)	SANITARY MANHOLE RIM EL.=1092.42 12"PPC(NW)=1080.87 12"PPC(NE)=1080.83
(SD5)	STORM SEWER MANHOLE RIM EL.=1098.74 30"RCP(SE)=1092.01 30"RCP(NW)=1092.34 24"CPP(SW)=1092.16	(SD12)	STORM SEWER MANHOLE RIM EL.=1090.03 24"RCP(NW)=1083.83 36"RCP(SW)=1082.53 42"RCP(NE)=1082.49	(SD19)	STORM SEWER MANHOLE RIM EL.=1100.83 15"RCP(NW)=1097.04 24"CPP(SW)=1095.59 24"CPP(NE)=1096.32	(SS5)	SANITARY MANHOLE RIM EL.=1102.16 12"PPC(NW)=1089.25 12"PPC(NE)=1089.05
(SD6)	STORM SEWER MANHOLE RIM EL.=1090.88 15"RCP(NE)=1087.09	(SD13)	STORM SEWER MANHOLE RIM EL.=1090.99 36"RCP(NW)=1084.89 36"RCP(SE)=1084.82	(SD14)	STORM SEWER MANHOLE RIM EL.=1096.87 24"RCP(NE)=1091.75 15"RCP(NW)=1092.07		
(SD7)	STORM SEWER MANHOLE RIM EL.=1089.42 30"RCP(NW)=1084.03 15"RCP(SW)=1084.61 30"RCP(SE)=1083.88						

LEGEND

-----	PROPERTY LINE
SD	EXISTING STORM SEWER
SS	EXISTING SANITARY SEWER
W	EXISTING WATER LINE
CATV	EXISTING CABLE/TELEVISION LINE
XXXX	EXISTING MAJOR CONTOUR
XXXX	EXISTING MINOR CONTOUR
=====	EXISTING CURB AND GUTTER

0' 15' 30' 60'

SCALE IN FEET

drawn by: JLC
checked by: EW
approved by: .
QA/CC by: .
project no.: 019-1124
drawing no.: 06.03.19
date: 06.03.19
CALL 811 SEVEN-TWO HOURS PRIOR TO
DIGGING, GRAVING OR EXCAVATING FOR
THE MARKING OF UNDERGROUND MEMBER UTILITIES.

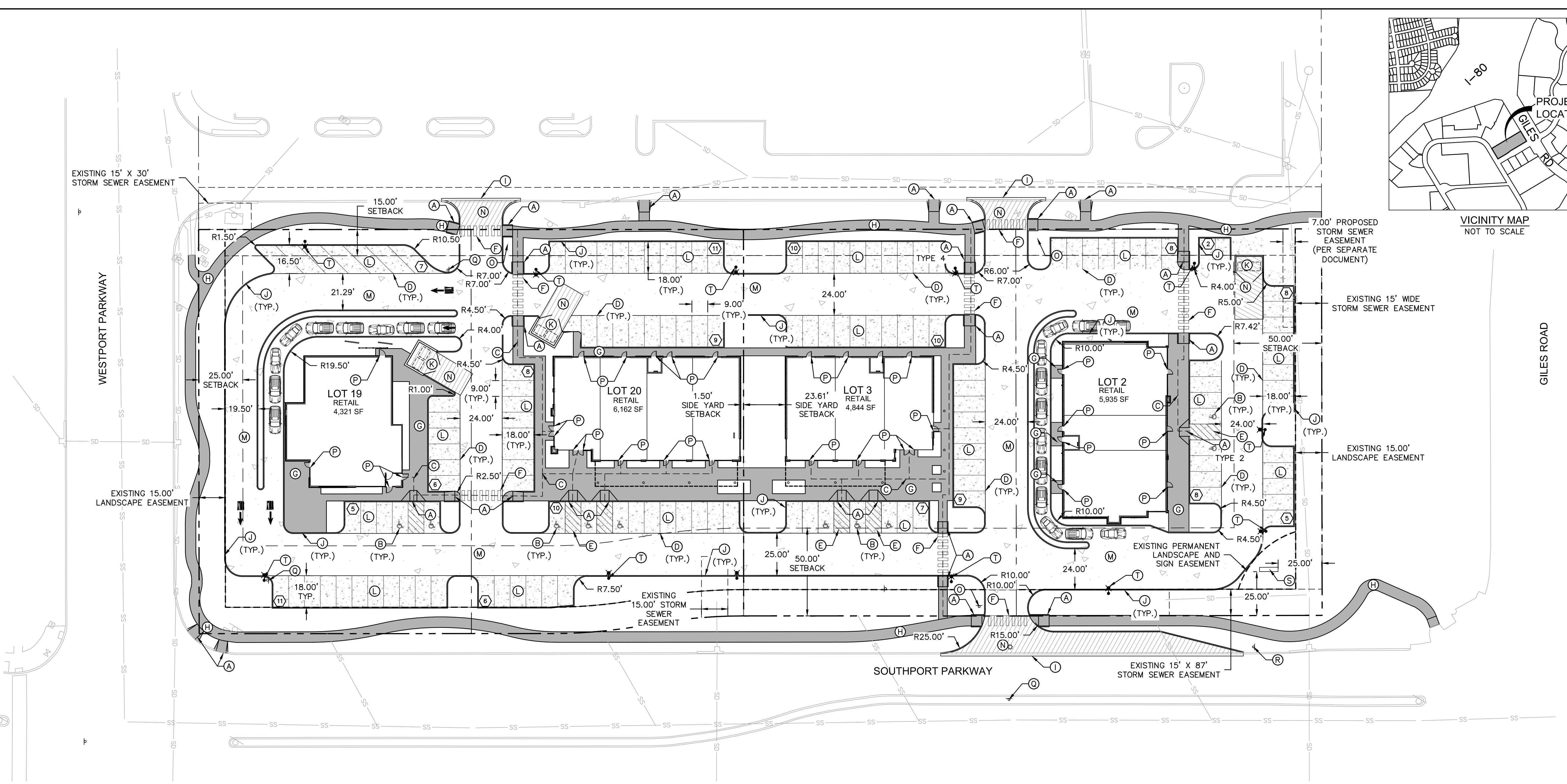
SHEET
C.1.1

EXISTING CONDITIONS & REMOVAL PLAN
PUD PLANS

BRIXMOR NORTH
LOTS 2, 3, 19, 20

LA VISTA, NEBRASKA

Olsson



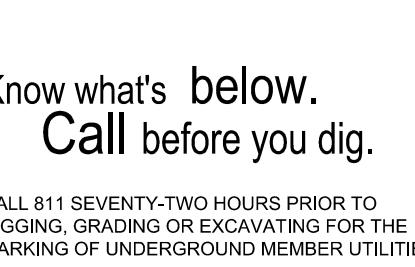
SITE KEY NOTES	
(A)	CONSTRUCT CURB RAMP PER ADA REGULATIONS AND LOCAL JURISDICTIONAL REQUIREMENTS. REFERENCE DETAIL SHEET AND CITY OF OMAHA STANDARD PLATE 500-82.
(B)	INSTALL ADA PARKING STALL AND ASSOCIATED STRIPING AND SIGNAGE PER ADA REGULATIONS AND LOCAL JURISDICTIONAL REQUIREMENTS. REFERENCE DETAIL SHEET.
(C)	PROPOSED ADA ACCESSIBLE ROUTE PER ADA REGULATIONS AND LOCAL JURISDICTIONAL REQUIREMENTS.
(D)	INSTALL 4-INCH WHITE PAVEMENT STRIPING. PAINT SHALL MEET OR EXCEED CITY OF OMAHA STANDARD SPECIFICATIONS (TYP.).
(E)	INSTALL 4-INCH WHITE PAVEMENT STRIPING AT 45°; 2-FEET O.C. PAINT SHALL MEET OR EXCEED CITY OF OMAHA STANDARD SPECIFICATIONS (TYP.).
(F)	INSTALL 24-INCH WHITE CROSSWALK PAVEMENT STRIPING. REFERENCE DETAIL SHEET. PAINT SHALL MEET OR EXCEED CITY OF OMAHA STANDARD SPECIFICATIONS (TYP.).
(G)	CONSTRUCT 4-INCH THICK P.C.C. SIDEWALK. REFERENCE DETAIL SHEET AND CITY OF OMAHA STANDARD PLATE 500-02.
(H)	CONSTRUCT 6-INCH THICK P.C.C. SIDEWALK IN PUBLIC R.O.W. 5' WIDE AND SPACED 6.5' OFF BACK OF CURB OF PUBLIC STREET (UNLESS OTHERWISE NOTED ON PLANS).
(I)	SAWCUT EXISTING PAVEMENT, FULL DEPTH AND CONSTRUCT THICKENED EDGE JOINT WITH DOWEL BAR CONNECTION PER CONCRETE WIDENING REINFORCED JOIN, PER CITY OF OMAHA STANDARD PLATE 500-60.
(J)	CONSTRUCT TYPE 'A' INTEGRAL CURB AND GUTTER PER CITY OF OMAHA STANDARD PLATE 500-52.
(K)	TRASH ENCLOSURE. REFERENCE ARCHITECTURAL PLANS. CONSTRUCT HEAVY DUTY PAVEMENT APRON AS SHOWN ON PLAN.
(L)	CONSTRUCT 6-INCH THICK STANDARD DUTY P.C. CONCRETE PAVEMENT. REFERENCE DETAIL SHEET.
(M)	CONSTRUCT 7-INCH THICK HEAVY DUTY P.C. CONCRETE PAVEMENT. REFERENCE DETAIL SHEET.
(N)	CONSTRUCT 9-INCH DRIVE ENTRANCE APRON HEAVY DUTY P.C. CONCRETE PAVEMENT. REFERENCE DETAIL SHEET.

(O)	INSTALL STOP SIGN PER MUTCD STANDARDS AND PER CITY OF OMAHA STANDARD PLATES 900-84 AND 900-85. REFERENCE DETAIL SHEET.
(P)	STRUCTURAL STOOP AND DOOR. REFERENCE ARCHITECTURAL PLANS FOR EXACT LOCATION, SIZE, AND SLOPE.
(Q)	INSTALL ONE WAY SIGN PER MUD STANDARDS AND PER CITY OF OMAHA STANDARD PLATES 906-02 AND 906-03. REFERENCE DETAIL SHEET.
(R)	INSTALL HOTEL DIRECTION/DELIVERY ROUTE SIGN PER MUD STANDARDS.
(S)	PROPOSED PROJECT DIRECTORY.
(T)	PROPOSED LIGHT POLE LOCATION.

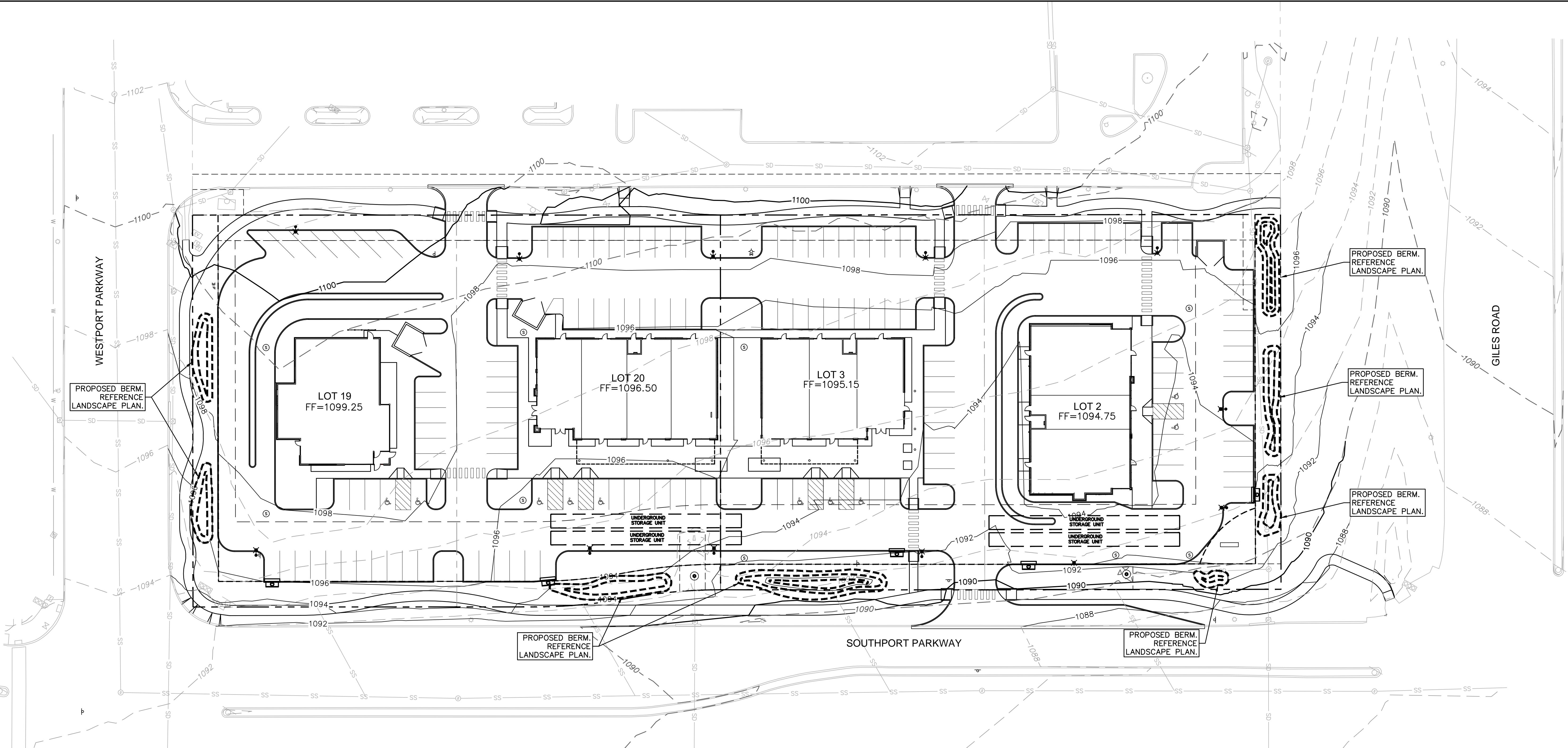
NOTES:

1. ALL RADII SHOWN ARE TO BACK-OF-CURB.
2. ALL RADII ARE 5' UNLESS OTHERWISE NOTED.
3. ALL DIMENSIONS SHOWN ARE TO BACK-OF-CURB.
4. ALL CURB AND GUTTER IS TYPE "A" INTEGRAL CURB PER CITY OF OMAHA STANDARD PLATE 1-52, UNLESS OTHERWISE NOTED.
5. CONTRACTOR TO SUBMIT SHOP DRAWINGS OF SIDEWALK JOINTING PLAN TO ENGINEER FOR APPROVAL PRIOR TO CONSTRUCTION.

SITE INFORMATION TABLE	
LEGAL DESCRIPTION:	BRIXMOR NORTH LOTS 2,3,19,20
EXISTING ZONING:	C-3 PUD WITH A GATEWAY CORRIDOR OVERLAY
PROPOSED ZONING:	C-3 PUD WITH A GATEWAY CORRIDOR OVERLAY
SETBACKS:	
FRONT YARD:	25'-0"
FRONT YARD: (PARKING PRESENT IN FRONT YARD)	50'-0"
SIDE YARD:	15'-0"
REAR YARD:	15'-0"
BUILDING COVERAGE:	15%
IMPERVIOUS COVERAGE:	25%
PARKING REQUIREMENTS:	
REQUIRED: (LOT 2)	27 STALLS (4.5 PER 1,000 SF; 5,935 SF BUILDING REQUIRES 27 STALLS)
PROVIDED: (LOT 2)	ON-SITE PARKING: 31 STALLS (5.22 CARS PER 1,000 SF)
REQUIRED: (LOT 3)	22 STALLS (4.5 PER 1,000 SF; 4,844 SF BUILDING REQUIRES 22 STALLS)
PROVIDED: (LOT 3)	ON-SITE PARKING: 36 STALLS (7.43 CARS PER 1,000 SF)
REQUIRED: (LOT 19)	19 STALLS (4.5 PER 1,000 SF; 4,321 SF BUILDING REQUIRES 19 STALLS)
PROVIDED: (LOT 19)	ON-SITE PARKING: 29 STALLS (6.71 CARS PER 1,000 SF)
REQUIRED: (LOT 20)	28 STALLS (4.5 PER 1,000 SF; 6,162 SF BUILDING REQUIRES 28 STALLS)
PROVIDED: (LOT 20)	ON-SITE PARKING: 36 STALLS (5.84 CARS PER 1,000 SF)
TOTAL REQUIRED:	96 STALLS
TOTAL PROVIDED:	141 STALLS
ACCESSIBLE PARKING:	
REQUIRED:	4
PROVIDED:	10



CALL 811 SEVEN-TWO HOURS PRIOR TO DIGGING, GRAVING OR EXCAVATING FOR THE MARKING OF UNDERGROUND MEMBER UTILITIES.



GRADING INFORMATION TABLE		
AREA	CONTOURS REPRESENT	CONTRACTOR TO GRADE TO
STREETS/INTERNAL DRIVES	FINISHED GROUND (TOP OF SLAB)	SUBGRADE (REF. SITE PLANS FOR PAVEMENT THICKNESS)
PARKING LOT PAVED AREAS	FINISHED GROUND (TOP OF SLAB)	SUBGRADE (REF. SITE PLANS FOR PAVEMENT THICKNESS)
BUILDING AREAS	FINISHED GROUND (FINISHED FLOOR ELEV.)	SUBGRADE (REF. ARCHITECTURAL PLANS FOR FLOOR SLAB AND SUBBASE THICKNESS)
ALL OTHER NON-PAVED/ NON-BUILDING AREAS	FINISHED GROUND	6" BELOW CONTOURS (FOR TOPSOIL)*

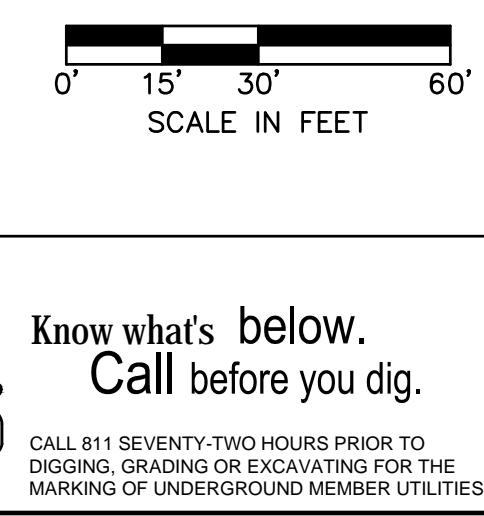
* NOTE: AFTER COMPLETION AND APPROVAL OF MASS GRADING BY ENGINEER, ENTIRE SITE SHALL RECEIVE 6" MIN. OF TOPSOIL FOR FINAL SEEDING AND STABILIZATION.

NOTES:

1. CONTOURS REPRESENT TOP OF SLAB ELEVATION IN PAVED AREAS AND FINISHED GRADE ELEVATION IN NON-PAVED AREAS, UNLESS OTHERWISE NOTED.
2. CONTRACTOR SHALL PROVIDE POSITIVE DRAINAGE AWAY FROM ALL BUILDING AND GARAGE PAD AREAS AND PAVEMENTS..
3. REFERENCE THE GRADING INFORMATION TABLE FOR CONTOUR INFORMATION.
4. REFERENCE THE FILL PLACEMENT / COMPACTION REQUIREMENTS TABLE FOR MINIMUM SUBGRADE PREPARATION REQUIREMENTS FOR VARIOUS AREAS OF THE PROJECT.
5. CONTOURS ARE SHOWN AT 2' INTERVALS FOR BOTH EXISTING AND PROPOSED.
6. DEVELOPMENT ON PRIVATE LOTS CREATING MORE THAN 5,000 SQUARE FEET OF IMPERVIOUS SURFACE FOR THE PURPOSE OF VEHICULAR TRAVEL AND PARKING SHALL PROVIDE ON-SITE STORM WATER MANAGEMENT FACILITIES FOR WATER QUALITY AS REQUIRED BY SECTION 154.19 OF THE LA VISTA MUNICIPAL CODE AND SHALL BE DESIGNED IN ACCORDANCE WITH THE APPLICABLE SECTIONS OF THE OMAHA REGIONAL STORM WATER DESIGN MANUAL, SUCH WATER QUALITY FACILITIES SHALL BE MAINTAINED BY BRIXMOR PROPERTY GROUP. MANAGEMENT OF THE 2-YEAR STORM RUNOFF PEAK FLOWS WILL BE PROVIDED BY THE CITY OWNED FACILITIES.

LEGEND

- 11XX — EXISTING MAJOR CONTOUR
- 11XX — EXISTING MINOR CONTOUR
- 11XX — PROPOSED MAJOR CONTOUR
- 11XX — PROPOSED MINOR CONTOUR
- 11XX — PROPOSED BERM



drawn by: JLC
 checked by: EW
 approved by: .
 OA/CC by: .
 project no.: 019-1124
 drawing no.: 06.03.19
 date: 06.03.19

SHEET
C3.1

REV. NO.	DATE	REVISIONS DESCRIPTION

REV. NO.	DATE	REVISIONS DESCRIPTION

REV. NO.	DATE	REVISIONS DESCRIPTION

REV. NO.	DATE	REVISIONS DESCRIPTION

REV. NO.	DATE	REVISIONS DESCRIPTION

REV. NO.	DATE	REVISIONS DESCRIPTION

REV. NO.	DATE	REVISIONS DESCRIPTION

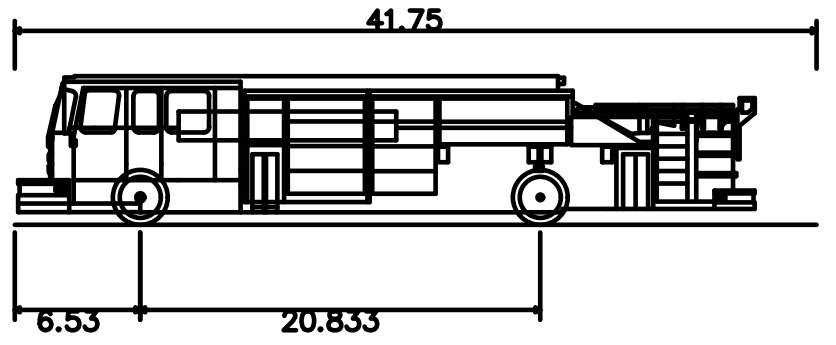
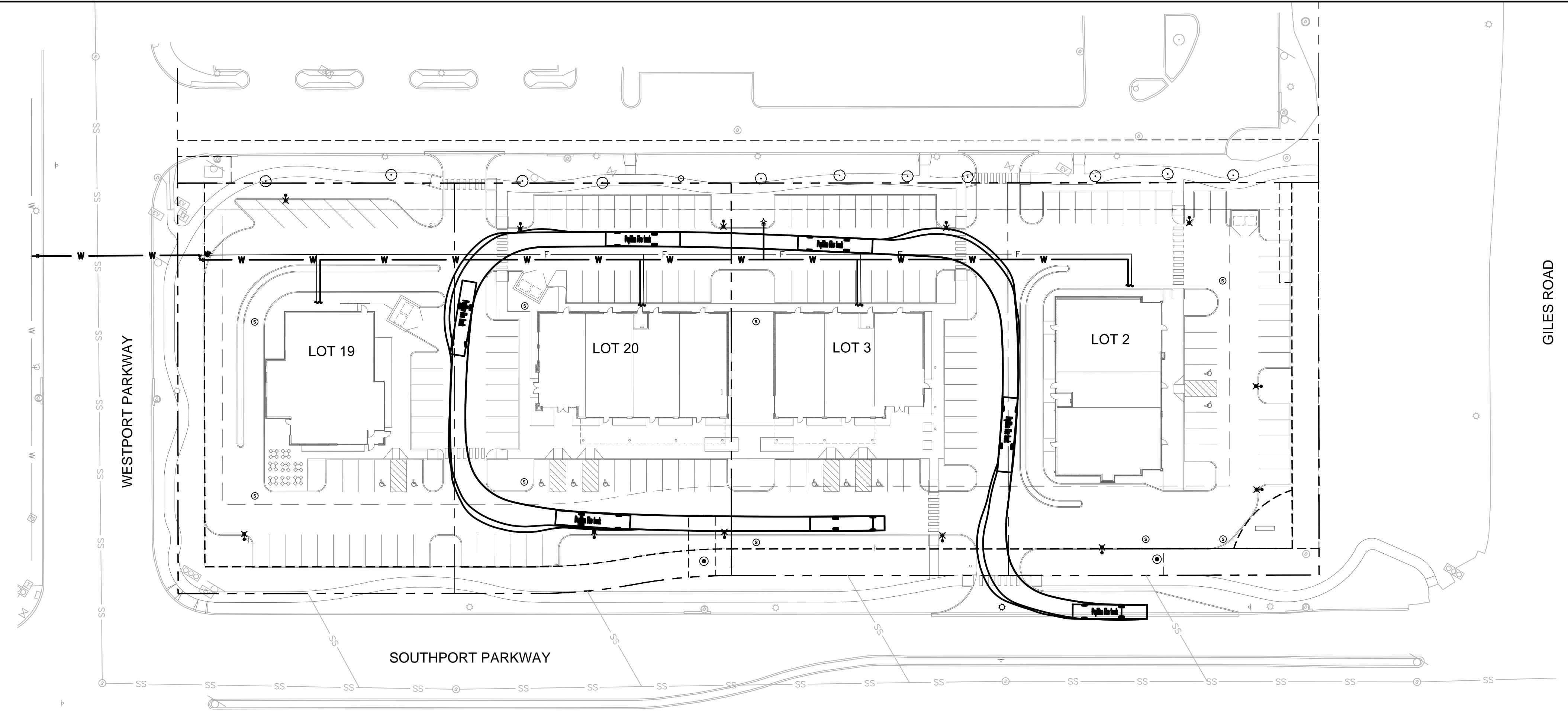
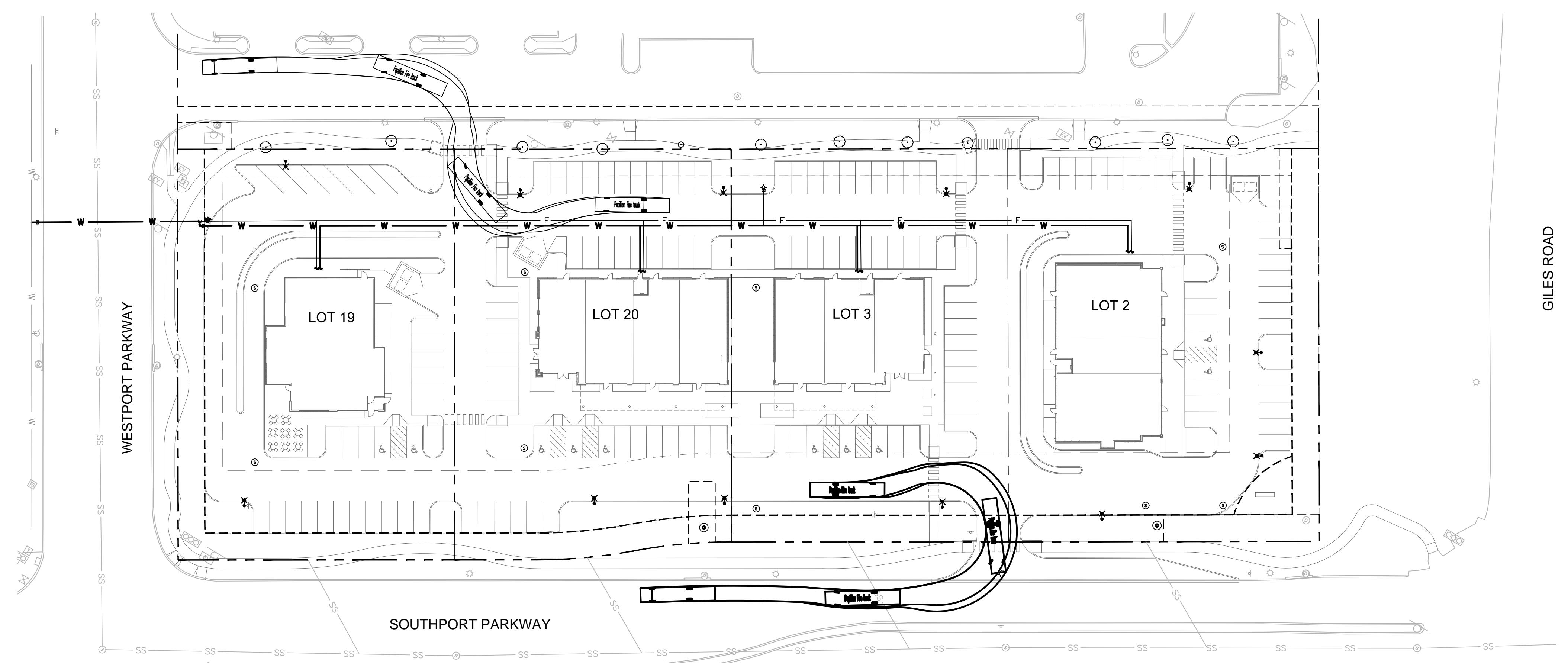
REV. NO.	DATE	REVISIONS DESCRIPTION

REV. NO.	DATE	REVISIONS DESCRIPTION

REV. NO.	DATE	REVISIONS DESCRIPTION

Olsson

DWG: F:\2019\1001-1500\019-1124\40-Design\AutoCAD\Preliminary Plans\Sheets\GNCV\C_FPR01_0191124.dwg
DATE: Oct 10, 2019 11:01am XREFS: c_ptblk_0191124
USER: ewilliams c_pbase_0191124
c_xbase_0191124 c_putil_0191124



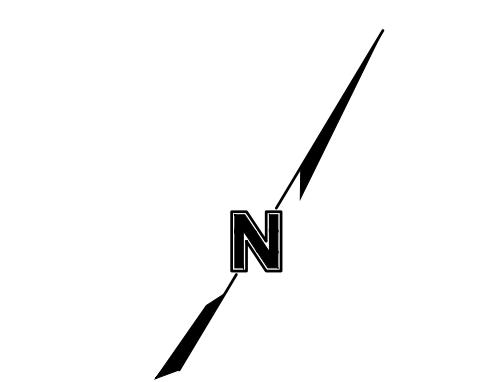
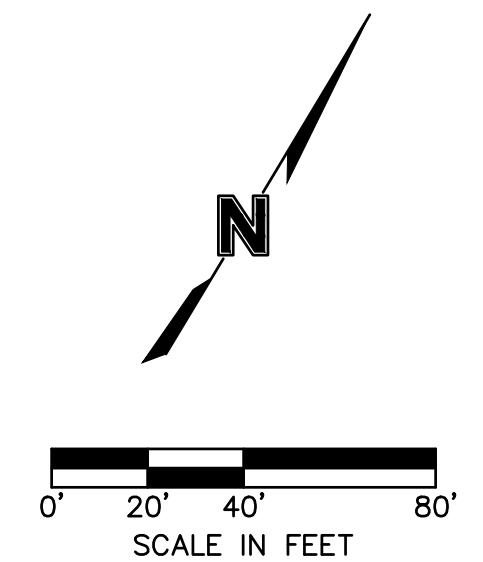
Papillion Fire truck
Overall Length
Overall Width
Overall Body Height
Min Body Ground Clearance
Track Width
Lock-to-lock time
Max Wheel Angle

41.750ft
8.000ft
7.496ft
0.746ft
8.142ft
5.00s
37.00•

ISSON

2111 South 67th Street, Suite 200
Omaha, NE 68106 TEL 402.341.1116 www.olsson.com

EMERGENCY VEHICLE ACCESS PLAN		NO.	DATE	REVISED DESCRIPTION
PUD PLANS				
BRIXMOR NORTH LOTS 2, 3 19, 20			2019	REVISIONS
LA VISTA, NEBRASKA				
Drawn by:	JLC			
Checked by:	EW			
Approved by:	-			
VQC by:	-			
Project no.:	019-1124			
Drawing no.:				
Date:	06.03.19			
SHEET C5.1				



Know what's below.
Call before you dig.

SHEET

C5.1



Brixmor North
Description
019-2182
Omaha, NE
10/10/2019

Common Area Estimated Costs

Disclaimer: This Opinion of Probable Construction Cost is subject to change based on several factors including, due diligence investigation findings, jurisdictional permitting and/or entitlement requirements, final design scope and changes in construction pricing. Since Olsson has no control over the cost of labor, materials, equipment or services furnished by others, or over the contractor(s)' methods of determining prices, or over competitive bidding or market conditions, Olsson's Opinion of Probable Cost provided for herein is made on basis of Olsson's experience and qualifications and represent Olsson's best judgment as an experienced and qualified professional engineer, familiar with the construction industry. Client acknowledges and agrees that Olsson cannot and does not guarantee proposals or bids and that actual total Project(s) or construction costs may reasonably vary from Olsson's Opinion of Probable Costs.

Item No.	Description	Qty	Units	Unit Price	Total	Notes
Project 4 Storm Sewer :						
101	Underground Detention	18,500.00	CF	\$15.00	\$277,500.00	
113	Perform CCTV Pipeline Inspection	370.00	LF	\$1.50	\$555.00	
114	Construct 15-36" RCP Class III Piping	370.00	LF	\$55.00	\$20,350.00	
115	Construct Water Quality Unit	2	EA	\$25,000.00	\$50,000.00	
116	Construct Curb Inlet - Type III	5	EA	\$5,000.00	\$25,000.00	
Engineering, Staking, Testing & Inspection					10%	\$37,340.50
					Subtotal	\$410,745.50
Project 5 Water:						
101	Construct Fire Hydrant	1	EA	\$2,000.00	\$2,000.00	
102	Construct 6" Fire Line	371	EA	\$55.00	\$20,405.00	
103	Construct 6" Water Main	95	EA	\$30.00	\$2,850.00	
104	Construct 2" Water Line	371	EA	\$55.00	\$20,405.00	
Engineering, Staking, Testing & Inspection					10%	\$4,566.00
					Subtotal	\$50,226.00
Project 6 Paving						
100	Mobilization	1.00	LS	\$10,000.00	\$10,000.00	
101	Pavement Marking/Signage	1.00	LS	\$5,000.00	\$5,000.00	
102	Subgrade Preparation	352.00	SY	\$3.00	\$1,056.00	
103	Sawcut-Full Depth	170.00	LF	\$6.00	\$1,020.00	
115	Construct 5" Concrete Sidewalk	18,010.00	SF	\$5.00	\$90,050.00	
116	Construct 9" Concrete Road	252.00	SY	\$65.00	\$16,380.00	
Engineering, Staking, Testing & Inspection					10%	\$11,350.60
					Subtotal	\$134,856.60
Construction Costs Plus Engineering Fees Subtotal						\$595,828.10
110% Construction Costs Plus Engineering Fees Total						\$655,410.91

BRIXMOR NORTH TRAFFIC IMPACT STUDY LA VISTA, NEBRASKA

Prepared For:

Slaggie Architects, Inc.
622 N 109th Plaza
Omaha, NE 68154

Prepared By:

Olsson
2111 S. 67th Street, Suite 200
Omaha, NE 68106



Olsson Project No. 019-1124
August 2019

olsson

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Appendix B	Existing Conditions Capacity Analysis Reports
Appendix C	2022 Background Capacity Analysis Reports
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1.0 INTRODUCTION AND OBJECTIVE

This report documents the results of analyses conducted for a proposed commercial development (Brixmor North) located in the northeast quadrant of Southport Parkway & Westport Parkway in La Vista, Nebraska. A map showing the general location of the proposed development is illustrated in **Figure 1**.

This study was conducted to evaluate current roadway conditions and to identify potential improvements to mitigate existing and future traffic issues. Three scenarios were analyzed as part of this study: Existing, 2022 Background, and 2020 plus Site (Opening Day) conditions. The year 2022 was chosen to represent the short-term horizon year with full build out and occupation of the development. Specific recommendations are included at the end of the report.

2.0 DATA COLLECTION

The data collection effort included obtaining peak hour turning movement counts and documentation of current roadway geometrics and traffic control.

2.1 Peak Hour Turning Movement Counts

Olsson coordinated intersection peak hour turning movement counts on Thursday, April 18th, 2019 at the following intersections:

- Southport Parkway & Giles Road
- Southport Parkway & Westport Parkway

All counts were collected at 15-minute intervals from 7:00am – 9:00am, and 4:00pm – 6:00pm. Counts include documentation of heavy vehicles at all count locations. Existing average daily traffic (ADT) volumes were estimated by applying a k-factor of 0.09 to turning movement volumes at each leg of Southport Parkway & Giles Road.

Existing peak hour traffic volumes are shown in **Figure 2**. Count data collected for this study can be found in **Appendix A**.

2.2 Field Review of Street Geometrics

A review of the existing roadway network including roadway type, general roadway geometrics, and traffic control device locations was completed as part of the data collection effort. Cross-section measurements and turn bay storage lengths were collected on each leg of the study intersections.

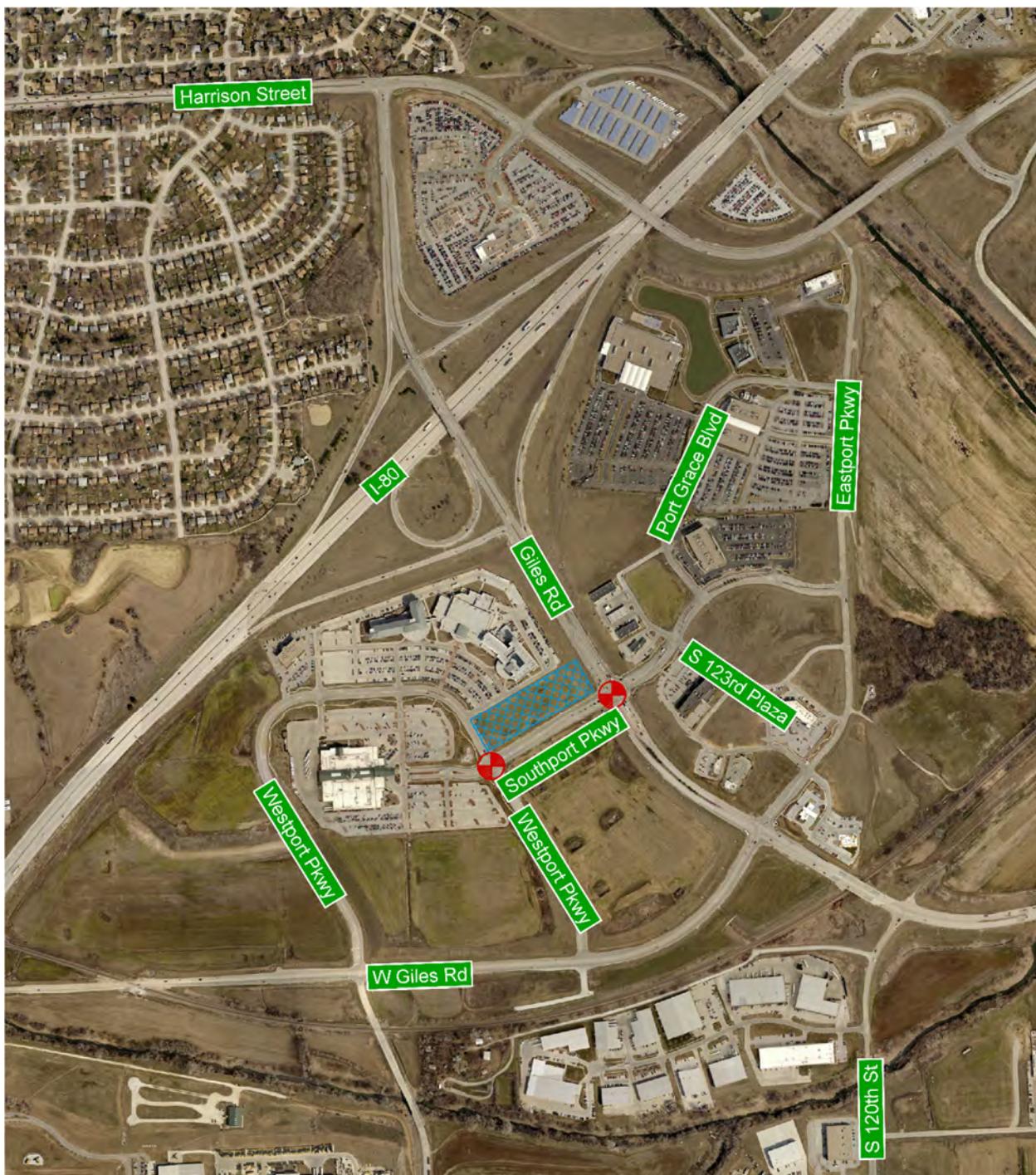


Figure 1. Vicinity Map

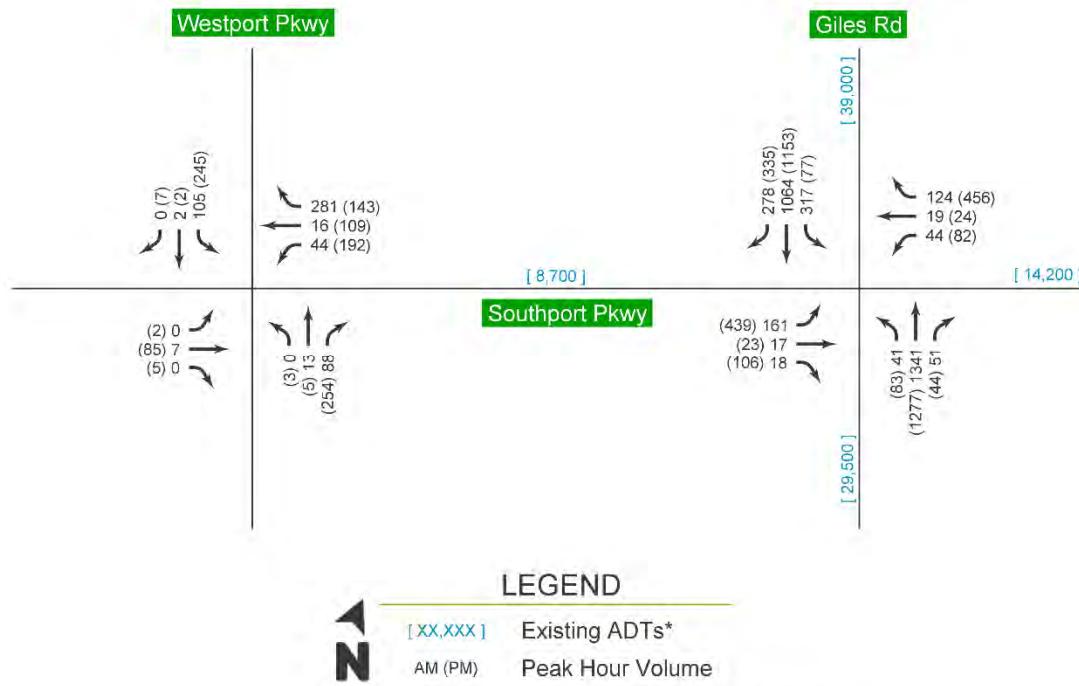


Figure 2. Existing Traffic Volumes

3.0 EXISTING CONDITIONS

Street conditions were evaluated to identify any existing deficiencies and to provide a baseline for comparison purposes.

3.1 Network Characteristics

Current roadway characteristics are summarized in **Table 1** below. Data for each roadway was acquired from aerial photography and the Metropolitan Area Planning Agency (MAPA) Federal Functional Classification map.

Table 1. Existing Roadway Characteristics.

Roadway	Section	Median Type	Posted Speed	Functional Classification
Giles Road	4-Lane	Divided	45 mph	Other Principal Arterial
Southport Parkway	4-Lane	Divided	25 mph	Local
Westport Parkway	5-Lane	TWLTL ¹	25 mph	Local

¹TWLTL = two-way left-turn lane.

The two study intersections, Southport Parkway & Giles Road and Southport Parkway & Westport Parkway, are both signalized. The intersection of Southport Parkway & Giles Road is built out with dual left-turn lanes at all approaches. Interstate 80 (I-80) is located approximately one-quarter mile north of Southport Parkway & Giles Road. There are sidewalks on the westbound approach of Southport Parkway & Giles Road.

The intersection of Southport Parkway & Westport Parkway has dual left turn lanes at southbound and westbound approaches. There are currently sidewalks on the west leg of the intersection, that extend north and south.

Existing lane configurations and traffic control are illustrated in **Figure 3**.

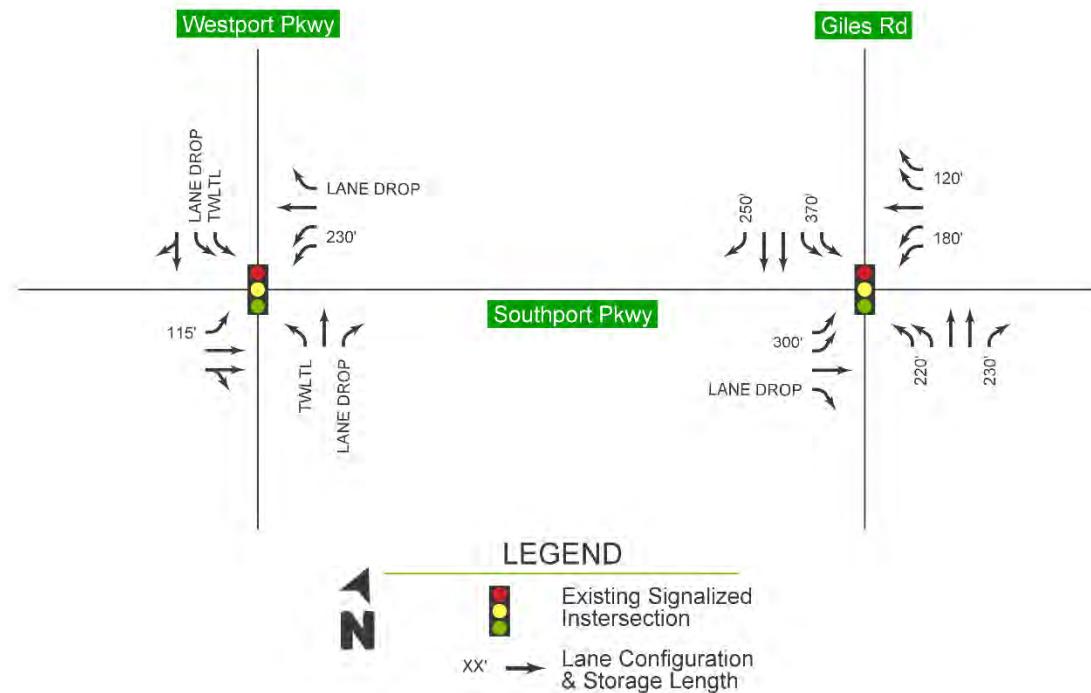


Figure 3. Existing Lane Configurations and Traffic Control

3.2 Existing Capacity Analysis

Capacity analyses were performed for the existing study intersections using the existing lane configurations and traffic control. Analyses were conducted using Synchro, Version 10.1 which is based on the Highway Capacity Manual (HCM), 6th Edition delay methodologies. For simplicity, the amount of control delay is equated to a grade or Level of Service (LOS) based on thresholds of driver acceptance. The amount of delay is assigned a letter grade A through F, LOS A representing little or no delay and LOS F representing very high delay. **Table 2** shows the delays associated with each LOS grade for signalized and unsignalized intersections. Signal timings were provided by the City of La Vista and these were incorporated in this and subsequent analyses.

Table 2. Intersection LOS Criteria.

Level-of-Service	Average Control Delay (seconds)	
	Signalized	Unsignalized
A	≤ 10	≤ 10
B	> 10-20	> 10-15
C	> 20-35	> 15-25
D	> 35-55	> 25-35
E	> 55-80	> 35-50
F	> 80	> 50

Highway Capacity Manual (HCM, 6th Edition)

Based on the capacity analysis, both signalized intersections operate at LOS D or better in both peak hours. Most individual movements operate at LOS D or better in both peak hours. There are several movements that operate at LOS E or LOS F in the AM and PM peak hours. Operations for these movements are detailed in **Table 3** below.

Table 3. Existing Capacity Analysis – Signalized LOS

Intersection	Movement	LOS AM (PM)	Delay (sec/veh) AM (PM)	Queue (ft) AM (PM)	V/C Ratio AM (PM)
Southport Parkway & Giles Road	EBL	E (E)	72 (61)	#117 (240)	0.80 (0.88)
	EBT	E (E)	59 (60)	0 (38)	0.37 (0.46)
	WBT	E (D)	56 (52)	41 (42)	0.24 (0.25)
	WBR	C (F)	25 (179)	25 (135)	0.32 (1.30)
	SBL	F (E)	91 (58)	#185 (55)	0.99 (0.56)

The vehicle-to-capacity (v/c) ratio for the westbound right-turning movement exceeds 1.0 in the PM peak hour which indicates these movements are oversaturated. In addition, the v/c ratio for the southbound left-turning movement approaches 1.0 in the AM peak hour indicating this movement is reaching capacity. All queue lengths are contained within existing storage lengths.

The Existing capacity analysis summary is illustrated in **Figure 4**. Detailed results may be found in **Appendix B**.

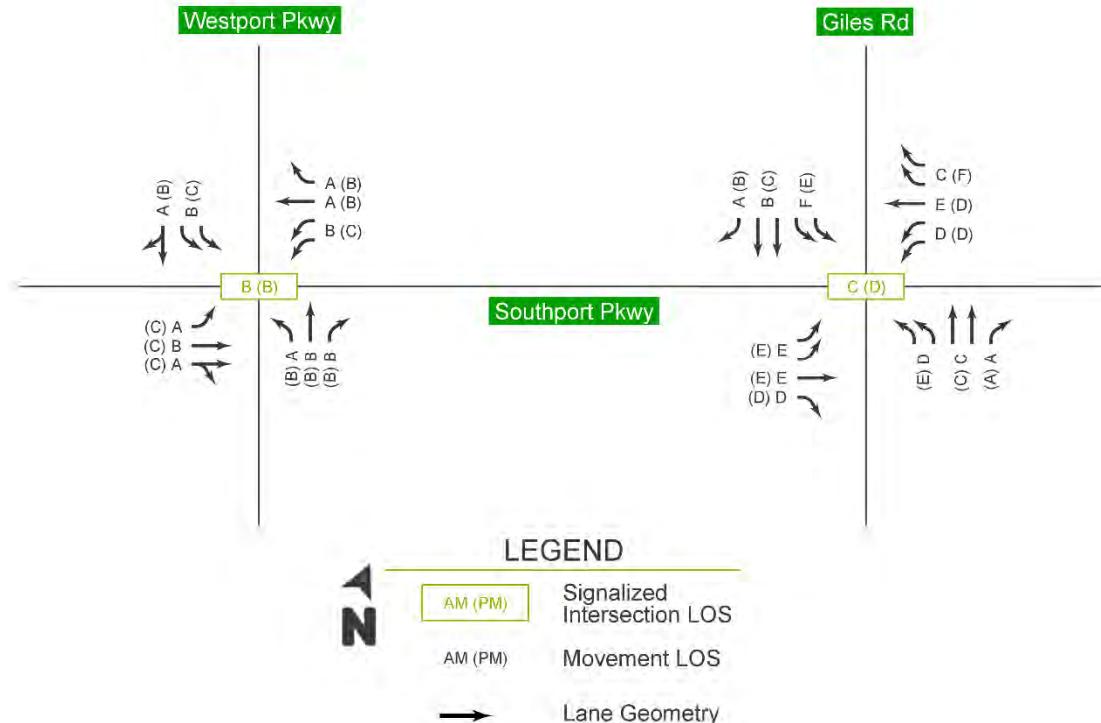


Figure 4. Existing Capacity Analysis

4.0 2022 BACKGROUND VOLUMES AND ANALYSIS

Year 2022 Background volumes were established to evaluate Opening Day conditions. 2022 Background volumes were applied to the study intersections and a capacity analysis was performed.

4.1 2022 Background Volumes

Year 2022 Background volumes were generated using an assumed annual growth rate for traffic volumes in the area. Olsson performed a traffic impact study in the area in the year 2017 for the La Vista Multi-Sport Complex (*Olsson* 2017). Based on traffic volumes and projections from the *Olsson* 2017 study, a two percent growth rate was applied to existing traffic volumes along Giles Road to develop future background volumes. Side street volumes along Southport Parkway are assumed to be development dependent. 2022 Background volumes are illustrated in **Figure 5**.

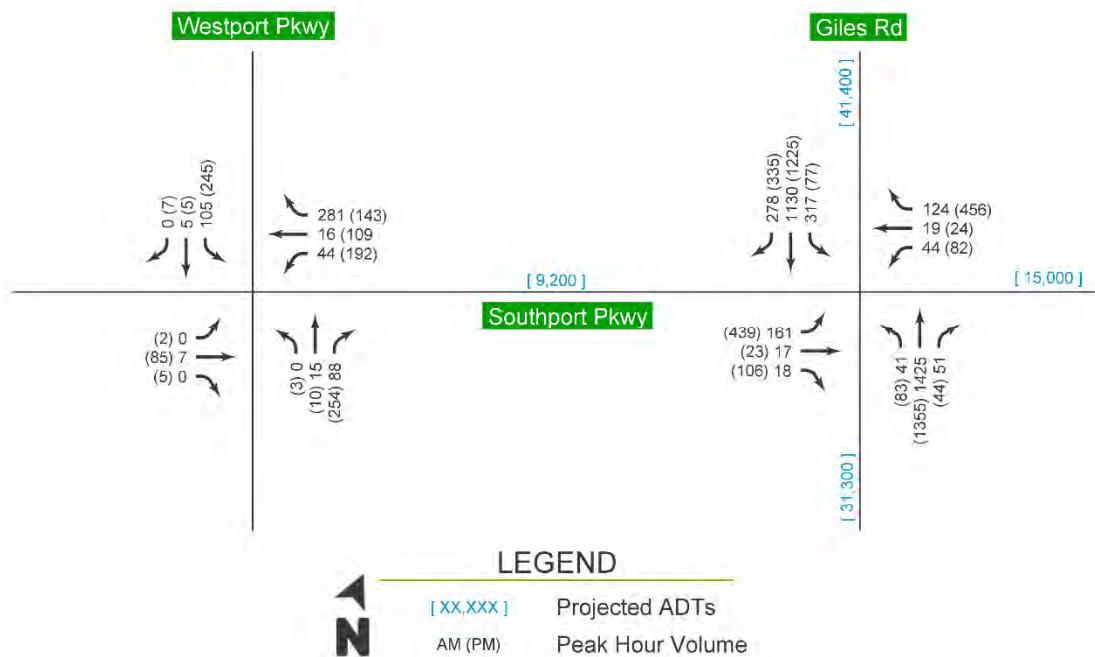


Figure 5. 2022 Background Traffic Volumes

4.2 2022 Background Capacity Analysis

Based on the capacity analysis, both signalized intersections are anticipated to operate at LOS D or better in both peak hours. Most turning movements are anticipated to operate at LOS D or better in either peak hour. Movements anticipated to operate at unacceptable levels of service are detailed in **Table 4** below.

Table 4. 2022 Background Capacity Analysis – Signalized LOS

Intersection	Movement	LOS AM (PM)	Delay (sec/veh) AM (PM)	Queue (ft) AM (PM)	V/C Ratio AM (PM)
Southport Parkway & Giles Road	EBL	E (E)	75 (61)	#120 (240)	0.82 (0.88)
	EBT	E (E)	59 (60)	0 (38)	0.37 (0.46)
	WBT	E (D)	56 (52)	41 (42)	0.24 (0.25)
	WBR	C (F)	25 (179)	24 (135)	0.32 (1.30)
	SBL	F (E)	95 (58)	#191 (55)	1.00 (0.56)

All queue lengths are anticipated to be contained within the current storage length for each of these movements. Similar to existing conditions, westbound right-turning vehicles are anticipated to have v/c ratios exceeding 1.0 in the PM peak hour. In addition, the southbound left-turning movement also experiences a v/c ratio of 1.0 in the AM peak hour which indicates this movement is oversaturated and may experience additional delay.

The 2022 Background conditions capacity analysis summary is illustrated in **Figure 6**. Detailed results may be found in **Appendix C**.

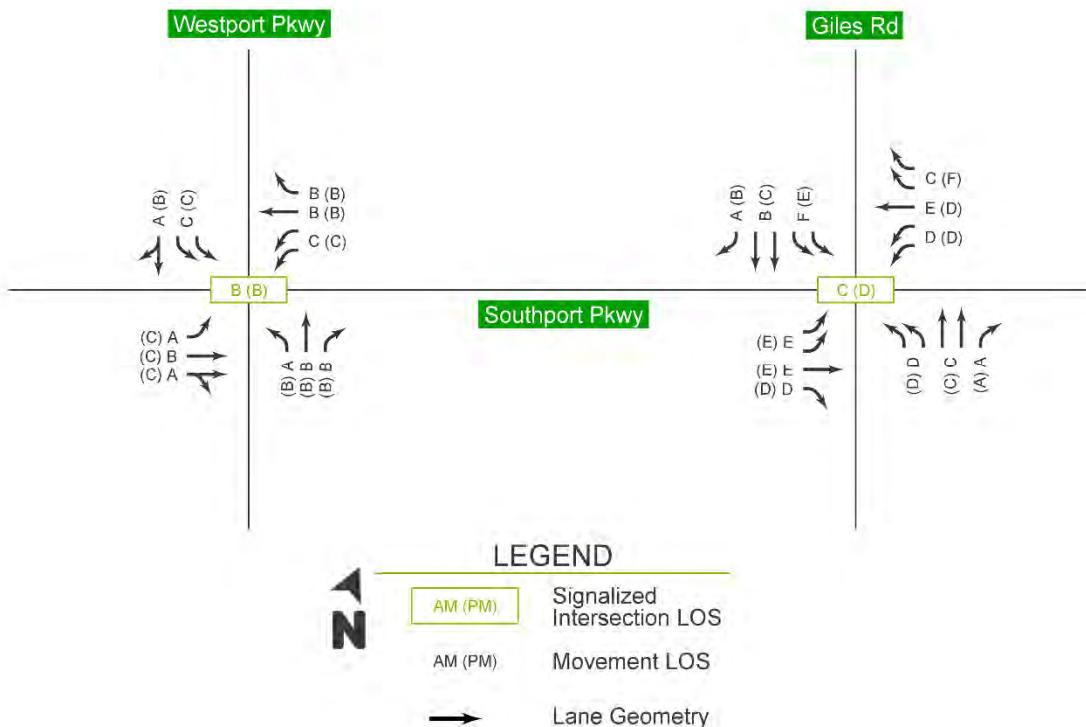


Figure 6. 2022 Background Capacity Analysis Summary

5.0 SITE CHARACTERISTICS

Trips anticipated to be associated with the Brixmor North Development were generated and applied to the study network to determine impacts to the existing roadway network. This study assumes one right-in, right-out (RIRO) connection to Southport Parkway. The location of this access is proposed to be approximately 300 feet west of Giles Road. The location was determined based on coordination with the City of La Vista and the client during site plan concept meetings. There is also an existing drive (Embassy Suites Drive) located approximately 250 feet northwest of Southport Parkway along Westport Parkway. This drive will be utilized for indirect access to the site. The site plan used for this study is provided in **Figure 7**.

5.1 Trip Generation

Trip generation characteristics were developed for the proposed site using rates found in the ITE Trip Generation Manual (10th Edition). Common Land Use Codes (LUC) are published with rates that can be applied to values related to the size of the proposed site to estimate the anticipated entering and exiting trips. LUC 820 Shopping Center, LUC 934 Fast-Food Restaurant with Drive-Through Window, and LUC 937 Coffee/Donut Shop with Drive-Through Window were used for the trip generation. The site is anticipated to be fully built out and occupied by year 2022.

Based on ITE methodologies, a pass-by reduction was applied to the site trips to account for trips made to the site while on the way to another destination. Pass-by trips include trips that are diverted from the roadways adjacent to the development that have direct access. Primary trips are trips made for the specific purpose of visiting the development.

A summary of the anticipated number of daily, AM peak hour, and PM peak hour trips for Brixmor North are shown in **Table 5**. This site is anticipated to generate a net of 4,567 daily trips, 402 AM peak hour trips, and 302 PM peak hour trips peak hour trips.

5.2 Trip Distribution

A trip distribution was developed based on existing travel patterns, surrounding land uses, and proximity to the interstate. The primary trip distribution, pass-by site trips, and total site trips are illustrated in **Figures 8-12**.



LEGEND



Signalized Intersection

Figure 7. Site Plan

Table 5. Trip Generation

ITE 10th Ed Code/Page	Lot	Land Use	Size	Daily Trip Generation							
				Trip Gen. Avg. Rate/Eq.	Daily Trips	Trip Distribution		Total Daily Trips			
						Enter	Exit	Enter	Exit		
934/201	A	Fast-Food Restaurant with Drive-Through Window	4,313	SF	470.95	2,031	50%	50%	1,016	1,016	
820/138	B	Shopping Center	6,307	SF	37.75	238	50%	50%	119	119	
820/138	C	Shopping Center	4,931	SF	37.75	186	50%	50%	93	93	
820/138	D	Shopping Center	2,500	SF	37.75	94	50%	50%	47	47	
820/138	D	Shopping Center	1,100	SF	37.75	42	50%	50%	21	21	
937/231	D	Coffee/Donut Shop with Drive-Through Window	2,408	SF	820.38	1,975	50%	50%	988	988	
Total					4,567			2,283	2,283		

ITE 10th Ed Code/Page	Lot	Land Use	Size	AM Peak Hour Trips							
				Trip Gen. Avg. Rate/Eq.	AM Peak Trips	Trip Distribution		Total AM Trips		Pass-by Reduction	Pass-by Trips
						Enter	Exit	Enter	Exit	Enter	Exit
934/202	A	Fast-Food Restaurant with Drive-Through Window	4,313	SF	40.19	173	51%	49%	88	85	49%
820/139	B	Shopping Center	6,307	SF	0.94	6	62%	38%	4	2	0%
820/139	C	Shopping Center	4,931	SF	0.94	5	62%	38%	3	2	0%
820/139	D	Shopping Center	2,500	SF	0.94	2	62%	38%	1	1	0%
820/139	D	Shopping Center	1,100	SF	0.94	1	62%	38%	1	0	0%
937/232	D	Coffee/Donut Shop with Drive-Through Window	2,408	SF	88.99	214	51%	49%	109	105	0%
Total					402			206	195	42	42
										164	153

ITE 10th Ed Code/Page	Lot	Land Use	Size	PM Peak Hour Trips							
				Trip Gen. Avg. Rate/Eq.	PM Peak Trips	Trip Distribution		Total PM Trips		Pass-by Reduction	Pass-by Trips
						Enter	Exit	Enter	Exit	Enter	Exit
934/203	A	Fast-Food Restaurant with Drive-Through Window	4,313	SF	32.67	141	52%	48%	73	68	50%
820/140	B	Shopping Center	6,307	SF	3.81	24	48%	52%	12	12	34%
820/140	C	Shopping Center	4,931	SF	3.81	19	48%	52%	9	10	34%
820/140	D	Shopping Center	2,500	SF	3.81	10	48%	52%	5	5	34%
820/140	D	Shopping Center	1,100	SF	3.81	4	48%	52%	2	2	34%
937/233	D	Coffee/Donut Shop with Drive-Through Window	2,408	SF	43.38	104	50%	50%	52	52	0%
Total					302			153	149	45	45
										108	104

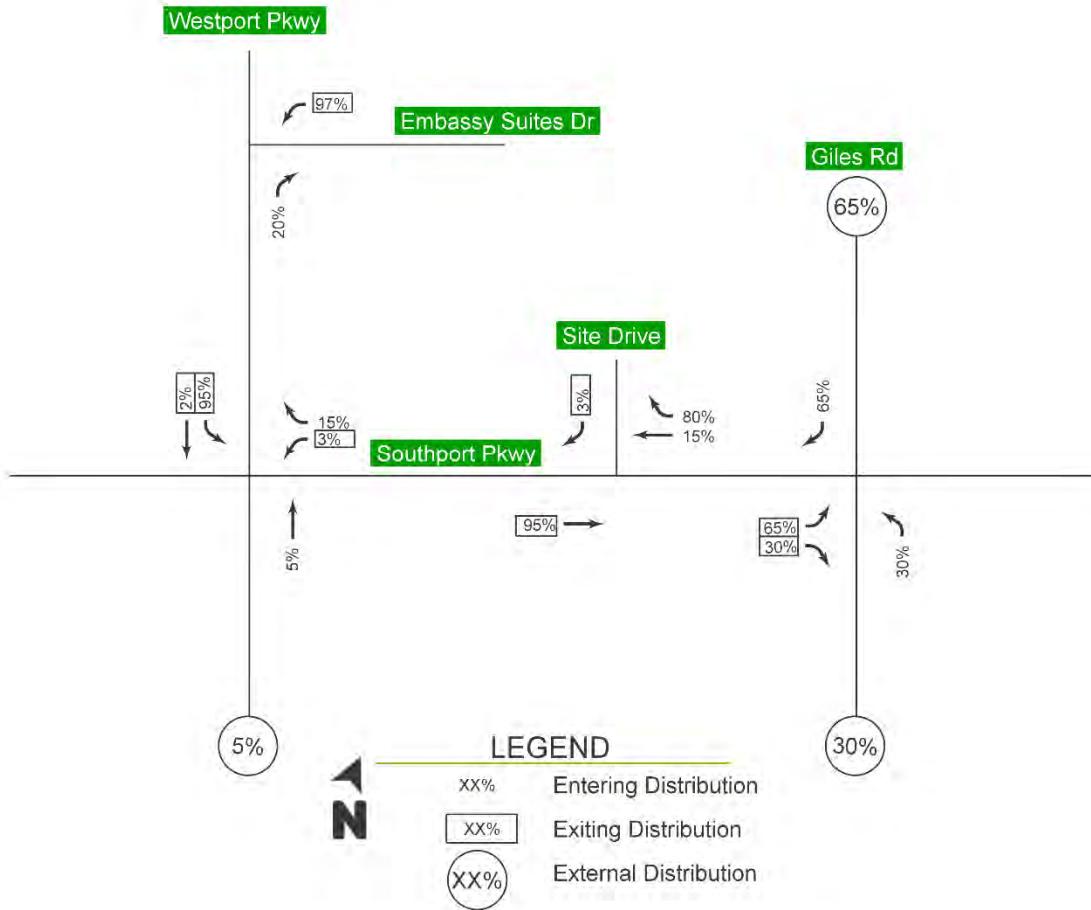


Figure 8. Trip Distribution

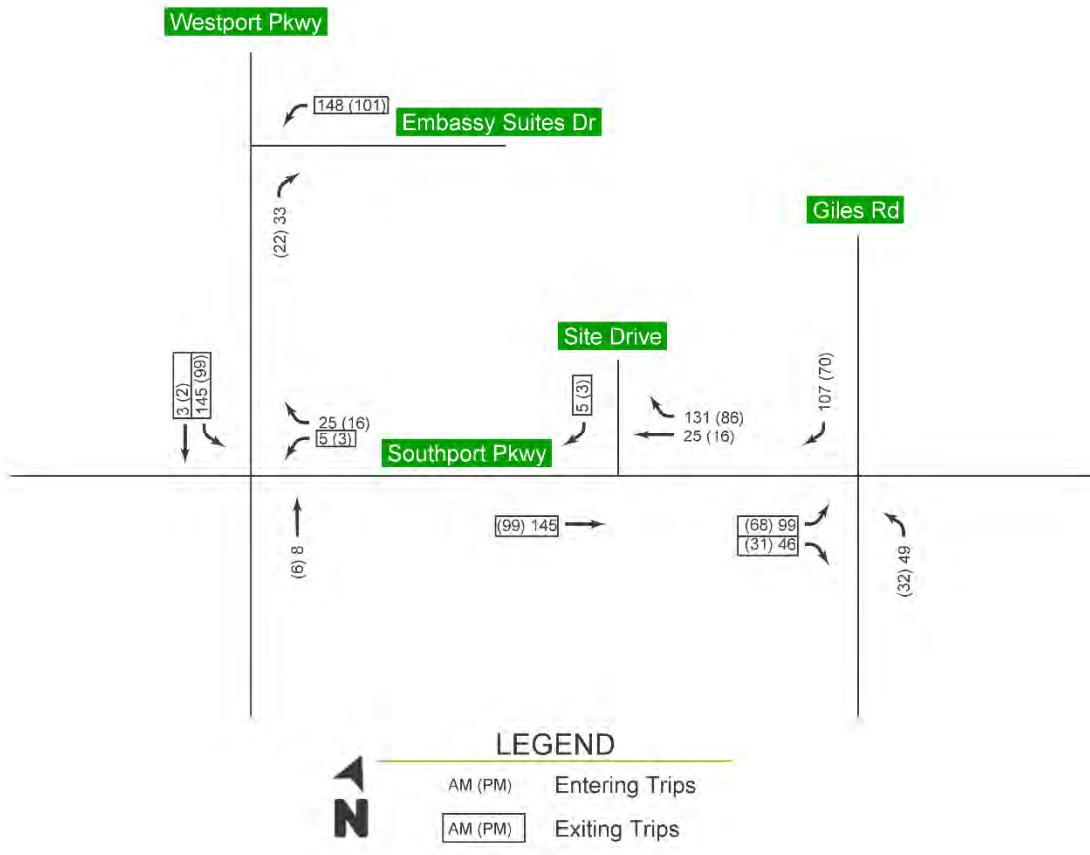


Figure 9. Primary Site Trips

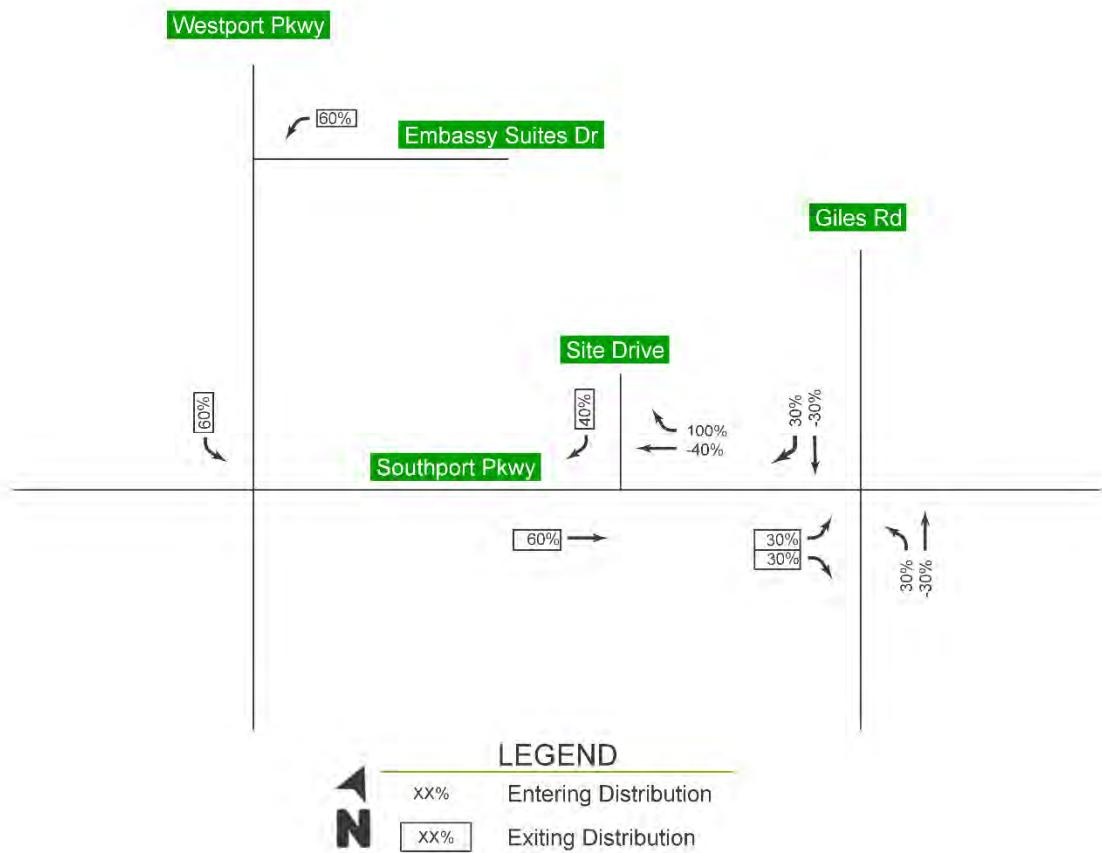


Figure 10. Pass-By Trip Distribution



Figure 11. Pass-By Site Trips

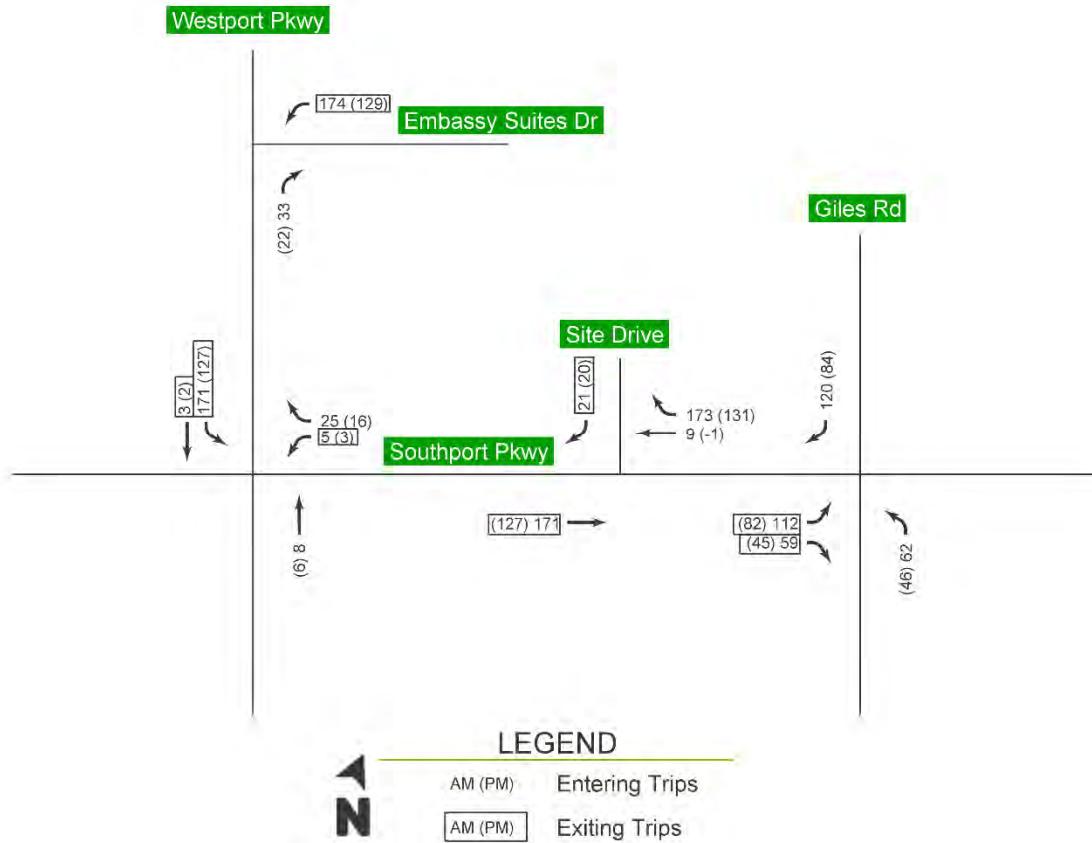


Figure 12. Total Site Trips

5.3 Site Traffic Circulation

The circulation of site traffic was analyzed to determine potential impacts on the roadway network.

Based on the location of the proposed Site Drive along Southport Parkway, there are concerns that traffic may use this drive for access to the adjacent hotel property. In addition, there are concerns delivery trucks using this access may need to wait for exiting vehicles in order to have adequate space to make a wide turn into the site, potentially stacking vehicles onto Southport Parkway. To address these concerns, the Southport Parkway access should include signage directing hotel traffic and delivery trucks to use Westport Parkway.

The current site plan proposes two land uses that include drive-through windows. Based on the current site plan, the available stacking for the coffee shop use is approximately 15 vehicles before it will begin to impede free flow parking lot movements along the east side of the lot. Due to the layout of the site, this is not anticipated to impact operations at any development access. The available stacking for the fast-food drive-through shown in the site plan is approximately nine vehicles. Queues longer than nine vehicles will block some movements at the westernmost development access off Embassy Suites Drive. In these cases, site traffic will be able to use the easternmost development access along Embassy Suites Drive.

6.0 FUTURE PLUS SITE ANALYSIS

The 2022 Background volumes were combined with the proposed site trips to develop the Opening Day volume scenario for capacity analysis purposes. The Opening Day peak hour volumes are shown in **Figure 13**.

6.1 Opening Day Capacity Analysis

Based on conversations with the City of La Vista, the construction of a westbound right-turn lane at the Site Drive along Southport Parkway will be required with site development and was included as part of the Opening Day analysis. All other intersections were analyzed with existing geometrics and intersection control.

Results of the capacity analysis indicate traffic operations along the north-south Giles Road mainline are anticipated to operate similar to 2022 Background conditions. There are several turning movements at Southport Parkway & Giles Road anticipated to operate at LOS E or LOS F in both peak hours that are detailed in **Table 6** below.

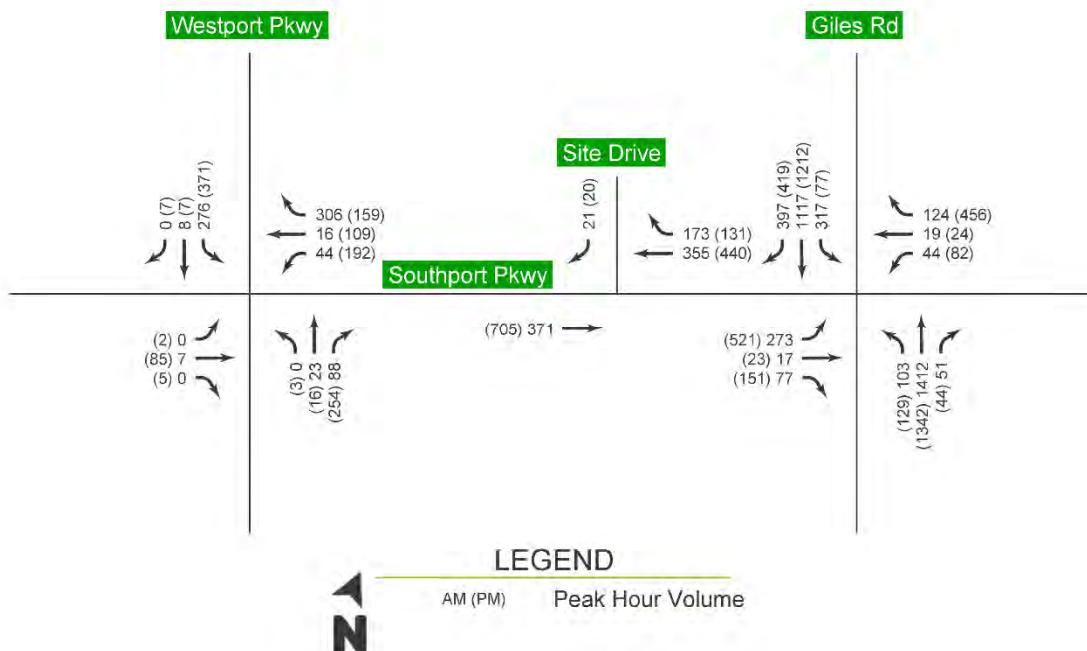


Figure 13. Opening Day Traffic Volumes

Table 6. Opening Day Capacity Analysis – Signalized LOS

Intersection	Movement	LOS AM (PM)	Delay (sec/veh) AM (PM)	Queue (ft) AM (PM)	V/C Ratio AM (PM)
Southport Parkway & Giles Road	EBL	F (E)	256 (66)	#238 (#319)	1.39 (0.91)
	EBT	E (D)	59 (54)	35 (43)	0.37 (0.26)
	EBR	D (E)	39 (59)	10 (82)	0.33 (0.87)
	WBT	E (D)	55 (52)	41 (42)	0.23 (0.25)
	WBR	D (F)	36 (177)	24 (135)	0.32 (1.30)
	NBL	D (E)	42 (57)	70 (88)	0.27 (0.59)
	SBL	F (E)	63 (58)	#191 (55)	1.00 (0.56)

Most queue lengths are anticipated to be contained in the current storage length for each of these movements. The 95th percentile queue length for the eastbound left-turning movement at Southport Parkway & Giles Road is anticipated to be approximately 320 feet in the PM peak hour which will extend into the taper of the existing storage bay. The eastbound left-turn lanes at Southport Parkway & Giles Road are currently back-to-back with the westbound left-turn lanes at Southport Parkway & Westport Parkway. There is currently no room to extend eastbound left-turn lanes.

There is an existing site drive approximately 250 feet northwest of the Southport Parkway & Westport Parkway intersection. The queue length for the southbound left-turning movement at this intersection is anticipated to be approximately 135 feet in the PM peak hour which will not extend beyond this drive. The Opening Day capacity analysis summary is illustrated in **Figure 14**. Detailed results may be found in **Appendix D**

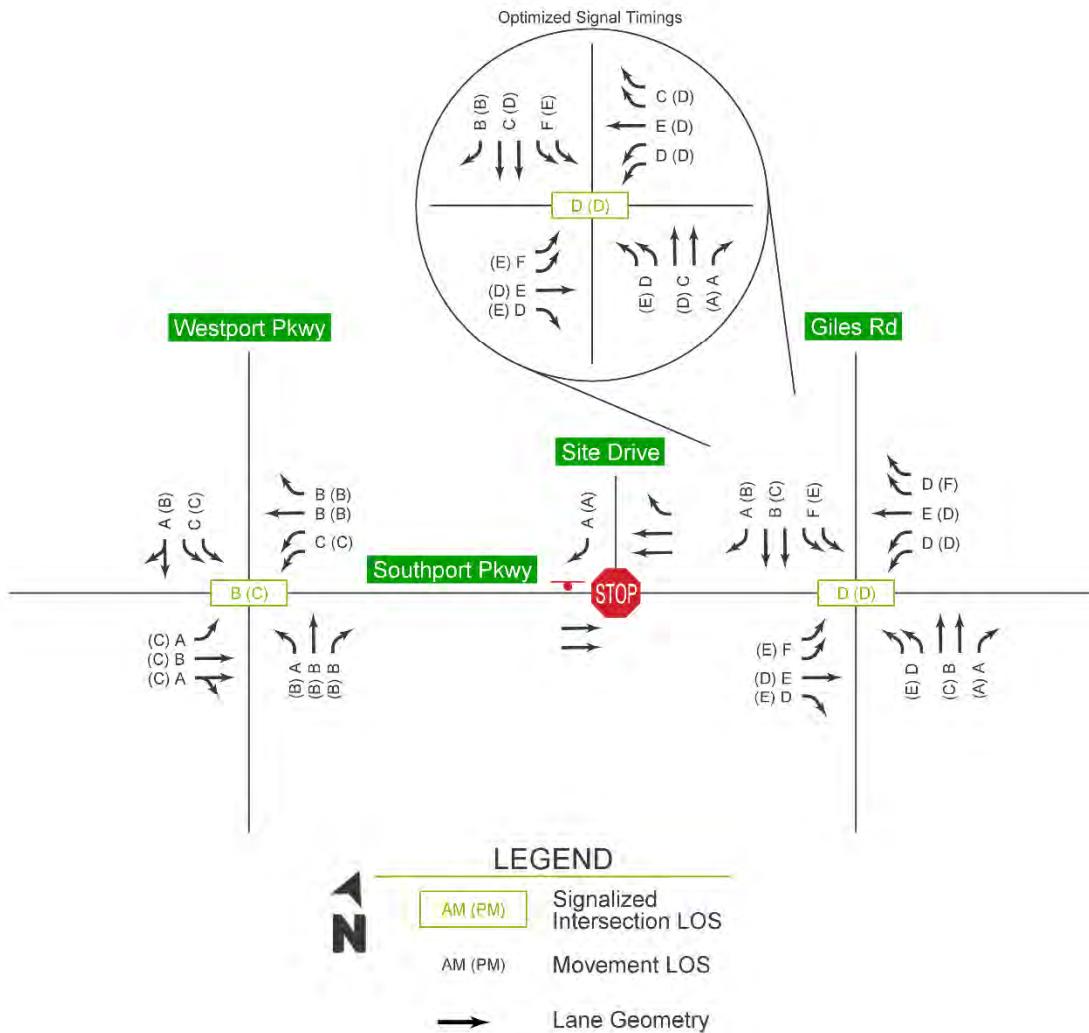


Figure 14. Opening Day Capacity Analysis

As part of the opening day capacity analysis, signal timings were evaluated for potential modification to improve overall traffic operations. At Southport Parkway & Giles Road, signal timings were optimized in both peak hours. Turning movement operations included in **Table 6** are shown in **Table 7** below for comparison purposes.

However, modifying signal timings at this intersection may impede progression along Giles Road. Northbound through movement queue lengths are anticipated to exceed 800 feet in the AM peak hour with optimized timings. Adjacent intersection signal timing coordination and north-south mainline progression along Giles Road should be considered before modifying signal timings at Southport Parkway & Giles Road. There are no signal timing recommendations for the intersection of Southport Parkway & Westport Parkway.

Table 7. Opening Day Capacity Analysis – Signalized LOS with Signal Timing Modifications

Intersection	Movement	LOS AM (PM)	Delay (sec/veh) AM (PM)	Queue (ft) AM (PM)	V/C Ratio AM (PM)
Southport Parkway & Giles Road	EBL	F (E)	89 (72)	153 (269)	0.95 (0.94)
	EBT	E (D)	59 (52)	34 (43)	0.37 (0.23)
	EBR	D (E)	39 (59)	9 (84)	0.34 (0.86)
	WBT	E (D)	55 (46)	41 (42)	0.23 (0.17)
	WBR	C (D)	23 (54)	8 (#154)	0.31 (0.98)
	NBL	D (E)	42 (63)	67 (#93)	0.28 (0.69)
	SBL	F (E)	89 (58)	#184 (56)	0.98 (0.56)

7.0 CONCLUSIONS & RECOMMENDED IMPROVEMENTS

This study documents the impacts of the proposed Brixmor North development located in the northeast quadrant of Southport Parkway & Westport Parkway in La Vista, Nebraska. This study was conducted to evaluate current roadway conditions and to identify potential improvements to mitigate existing and future traffic issues. Three scenarios were analyzed at study intersections: Existing, 2022 Background, and 2022 plus Site (Opening Day) conditions. Year 2022 coincides with the full build out of the site.

Trips associated with the development were generated based on ITE methodology. Site trips were combined with 2022 Background volumes and capacity analyses were performed for the Opening Day volume scenario.

Traffic is generally anticipated to operate with acceptable delay. However, several minor street turning movements at the intersection of Southport Parkway & Giles Road are anticipated to experience unacceptable delay or long queues in short-term future conditions. As traffic volumes increase along Giles Road and developments in the area build out, signal timings in the area should be considered for modification to improve traffic operations through the Giles Road corridor. There are no recommended signal timing modifications at study intersections currently.

The City of La Vista is requiring a westbound right-turn lane to be installed with the site at the RIRO site drive along Southport Parkway which is located approximately 300 feet west of Giles Road. This turn lane is proposed to have 75 feet of storage length. There should be a "One Way" sign (MUTCD R6-1) installed in the median along Southport Parkway with the construction of this drive. There should also be signage installed prior to the site drive directing hotel and delivery traffic to use Westport Parkway to reduce potential congestion at this site access. In addition, a "No U-Turn" sign (MUTCD R3-4) should be installed at the intersection of Westport Parkway & Southport Parkway to restrict westbound u-turning movements.

